

PROJECT ID: HWY24-02
WITH: N/A

COUNTY: WASHINGTON

ORDER OF SHEETS

| | | |
|-------------|---|---------------------------------|
| Section No. | 1 | Title |
| Section No. | 2 | Typical Sections and Details |
| Section No. | 3 | Miscellaneous Quantities |
| Section No. | 4 | Right of Way Plat |
| Section No. | 5 | Plan and Profile |
| Section No. | 6 | WisDOT Standard Detail Drawings |
| Section No. | 7 | WisDOT Sign Plates |
| Section No. | 8 | Structure Plans |
| Section No. | 9 | Computer Earthwork Data |
| Section No. | 9 | Cross Sections |

TOTAL SHEETS =

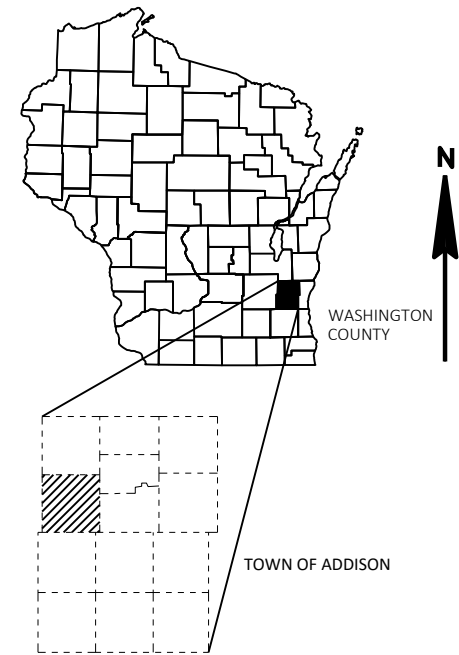
WASHINGTON COUNTY HIGHWAY DEPARTMENT

PLAN OF PROPOSED IMPROVEMENT

CTH S RECONSTRUCTION CTH R - CTH W TOWN OF ADDISON

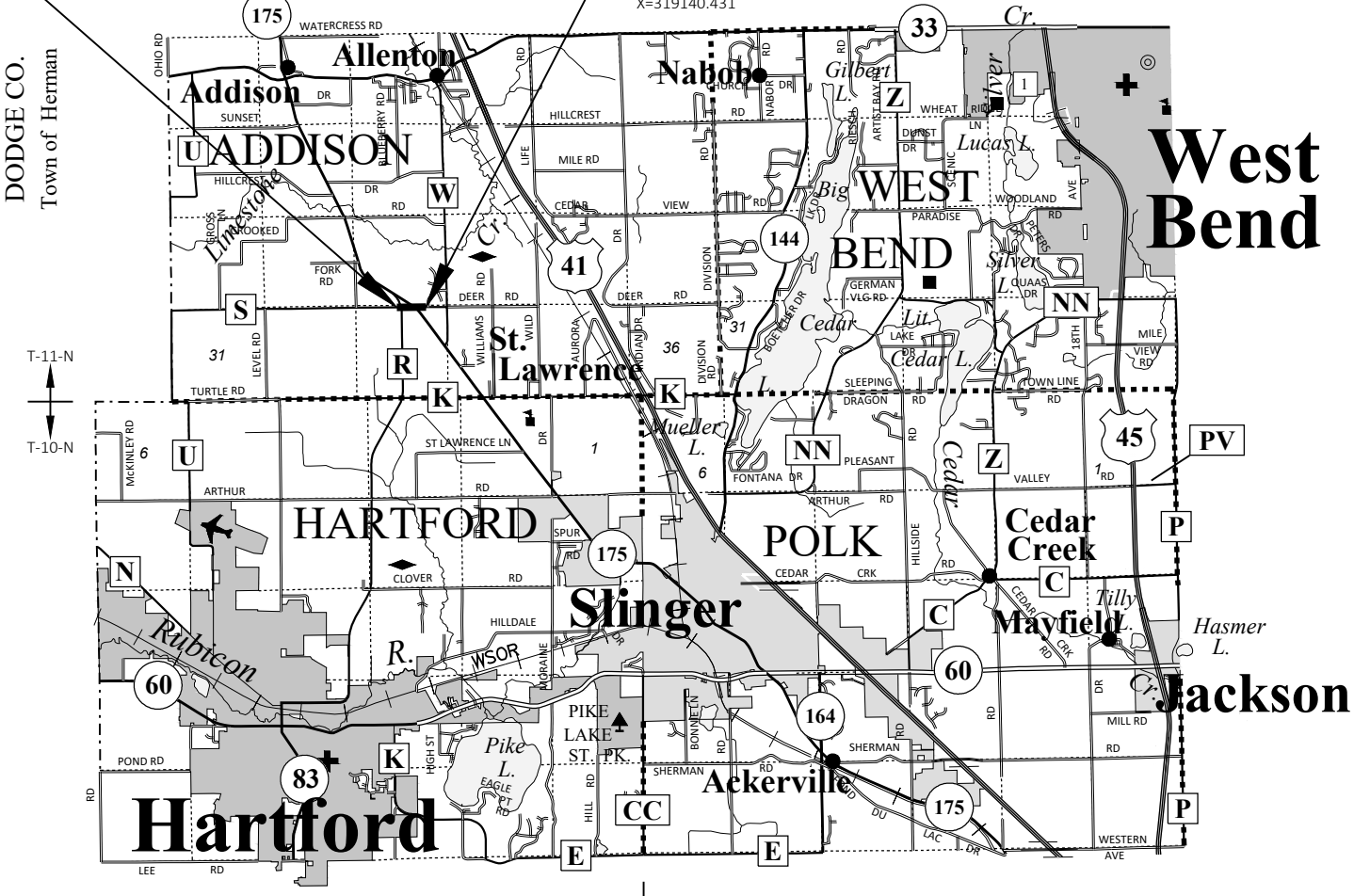
PRELIMINARY
05/11/2023

PROJECT NUMBER
HWY24-02



BEGIN PROJECT
STA 197+00.00
Y=170136.975
X=317425.283

END PROJECT
STA 214+60.00
Y=170106.561
X=319140.431



CONVENTIONAL SYMBOLS

| PLAN | |
|--------------------------------|--|
| CORPORATE LIMITS | |
| PROPERTY LINE | |
| LOT LINE | |
| LIMITED HIGHWAY EASEMENT | |
| EXISTING RIGHT OF WAY | |
| PROPOSED OR NEW R/W LINE | |
| SLOPE INTERCEPT | |
| REFERENCE LINE | |
| EXISTING CULVERT | |
| PROPOSED CULVERT (Box or Pipe) | |
| COMBUSTIBLE FLUIDS | |
| MARSH AREA | |
| WOODED OR SHRUB AREA | |

| PROFILE | |
|---|--|
| GRADE LINE | |
| ORIGINAL GROUND | |
| MARSH OR ROCK PROFILE (To be noted as such) | |
| SPECIAL DITCH | |
| GRADE ELEVATION | |
| CULVERT (Profile View) | |
| UTILITIES | |
| ELECTRIC | |
| FIBER OPTIC | |
| GAS | |
| SANITARY SEWER | |
| STORM SEWER | |
| TELEPHONE | |
| WATER | |
| UTILITY PEDESTAL | |
| POWER POLE | |
| TELEPHONE POLE | |

ACCEPTED FOR
WASHINGTON COUNTY

Date _____

SCOTT SCHMIDT
HIGHWAY COMMISSIONER

ORIGINAL PLANS PREPARED BY

G GREMMER & ASSOCIATES, INC.
CONSULTING ENGINEERS

Stevens Point • Fond du Lac
93 South Pioneer Road, Suite 300
Fond du Lac, WI 54805
(920) 924-5720

DATE: _____

BENJAMIN L. OITZINGER, PE

GENERAL NOTES

WETLAND LIMITS ARE SHOWN ON THE PLANS. CONTRACTOR SHALL LIMIT CONSTRUCTION ACTIVITIES TO WORK WITHIN THE SLOPE INTERCEPTS IN THE WETLAND AREAS.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY UTILITY WHICH IS NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

A VERTICAL SAW CUT SHALL BE MADE THROUGH EXISTING DRIVEWAYS, SIDEWALKS AND PAVEMENTS AT THE REMOVAL LIMITS.

SAWCUT LOCATIONS SHOWN ON THE PLANS ARE SUBJECT TO ADJUSTMENT BY THE ENGINEER IN THE FIELD.

WHEN THE QUANTITY OF THE ITEMS OF BASE AGGREGATE DENSE, HMA PAVEMENT OR ASPHALTIC SURFACE IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE MATERIAL SHOWN ON THE PLAN IS APPROXIMATE, AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

THE EXACT LOCATION AND LAYOUT OF PRIVATE ENTRANCES IS TO BE DETERMINED IN THE FIELD BY THE ENGINEER.

CLEARING AND GRUBBING SHALL BE LIMITED TO THE LIMITS WITHIN THE SLOPE INTERCEPTS AND AS DIRECTED BY THE ENGINEER.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

TOPSOIL, FERTILIZER, SEED AND MULCH OR EROSION MAT AS SHOWN IN PLANS OR AS DIRECTED BY THE ENGINEER SHALL BE PLACED ON ALL DISTURBED AREAS, EXCLUSIVE OF THE AREA OCCUPIED BY THE NEW PAVEMENTS, SIDEWALKS, ENTRANCES, AND RELATED STRUCTURES.

AREAS DISTURBED OUTSIDE OF THE GRADING LIMITS ARE TO BE RESTORED AT THE RESPONSIBILITY OF THE CONTRACTOR.

SECTIONS AS SHOWN ON THE CROSS-SECTIONS INCLUDE THE THICKNESS OF TOPSOIL WHERE REQUIRED.

CONTRACTOR SHALL VERIFY EXISTING PIPE SIZES, MATERIALS AND INVERT ELEVATIONS WHEN CONNECTING NEW STORM SEWER INTO EXISTING PIPES PRIOR TO MANUFACTURING INLETS AND MANHOLES.

ROTATE MANHOLE COVERS TO MATCH LANE LINES OR CENTER OF LANE AS DIRECTED BY THE ENGINEER IN THE FIELD.

REMOVAL OF ASPHALTIC PAVEMENT SHALL BE MEASURED AND PAID FOR AS EXCAVATION COMMON.

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING, OR PARKING LANE.

ABBREVIATIONS

| | |
|-------|---|
| AEW | APRON ENDWALL |
| AGG | AGGREGATE |
| AH | AHEAD |
| ASP | ASPHALT |
| BK | BACK |
| BAD | BASE AGGREGATE DENSE |
| BM | BENCH MARK |
| CC | CENTER OF CURVATURE |
| CE | COMMERCIAL ENTRANCE |
| C&G | CURB AND GUTTER |
| C/L | CENTER OR CONSTRUCTION LINE |
| CONC | CONCRETE |
| CP | CULVERT PIPE |
| CPCM | CULVERT PIPE CORRUGATED METAL |
| CPCS | CULVERT PIPE CORRUGATED STEEL |
| CPRC | CULVERT PIPE REINFORCED CONCRETE |
| CS | CURVE SPIRAL, THE POINT OF CHANGE IN ALIGNMENT FROM CURVE TO SPIRAL |
| CSD | CONCRETE SURFACE DRAIN |
| CY | CUBIC YARD |
| D | DEGREE OF CURVE |
| Δ | DELTA |
| DISCH | DISCHARGE |
| E | EXTERNAL DISTANCE FROM MIDPOINT OF CIRCULAR CURVE FROM ANGLE INTERSECTION |
| EB | EASTBOUND |
| ELEV | ELEVATION |
| FE | FIELD ENTRANCE |
| HMA | HOT MIX ASPHALT |
| HP | HIGH POINT |
| HT | HEIGHT |
| INV | INVERT |
| L | LENGTH OF CURVE |
| LHF | LEFT HAND FORWARD |
| LP | LOW POINT |
| LS | LENGTH OF SPIRAL |
| LT | LEFT |
| MAX | MAXIMUM |
| MIN | MINIMUM |
| M/L | MATCHLINE |
| NB | NORTHBOUND |
| NC | NORMAL CROWN |
| NOM | NOMINAL |
| NORM | NORMAL |
| PAVT | PAVEMENT |
| PC | POINT OF CURVE |
| PCC | POINT OF COMPOUND CURVE |
| PE | PRIVATE ENTRANCE |
| PI | POINT OF INTERSECTION |
| PLE | PERMANENT LIMITED EASEMENT |
| PT | POINT OF TANGENT |
| R | RADIUS OF CURVE |
| R/L | REFERENCE LINE |
| R/W | RIGHT OF WAY |
| RC | REVERSE CROWN |
| RCAEW | APRON ENDWALL FOR CULVERT PIPE REINFORCED CONCRETE |
| RCP | REINFORCED CONCRETE PIPE |
| REQ'D | REQUIRED |
| RHF | RIGHT HAND FORWARD |
| RO | RUN OFF LENGTH |
| RT | RIGHT |
| SALV | SALVAGED |
| SB | SOUTHBOUND |
| SC | SPIRAL CURVE, THE POINT OF CHANGE IN ALIGNMENT FROM SPIRAL TO CURVE |
| SDD | STANDARD DETAIL DRAWING |
| SE | SUPER ELEVATION |
| SEG | SEGMENT |
| SF | SQUARE FOOT |
| SS | STORM SEWER |
| ST | SPIRAL TANGENT, THE POINT OF CHANGE IN ALIGNMENT FROM SPIRAL TO TANGENT |
| STA | STATION |
| SY | SQUARE YARD |
| T | TANGENT LENGTH |
| TLE | TEMPORARY LIMITED EASEMENT |
| TS | TANGENT SPIRAL, THE POINT OF CHANGE IN ALIGNMENT FROM TANGENT TO SPIRAL |
| TYP | TYPICAL |
| V | VELOCITY OR DESIGN SPEED |
| VC | VERTICAL CURVE |
| VCL | VERTICAL CURVE LENGTH |
| VPC | POINT OF VERTICAL CURVE |
| VPI | POINT OF VERTICAL INTERSECTION |
| VPRC | POINT OF VERTICAL REVERSE CURVE |
| VPT | POINT OF VERTICAL TANGENT |
| WB | WESTBOUND |
| WCL | WISCONSIN CENTRAL LTD. |

ORDER OF SECTION 2 SHEETS

- GENERAL NOTES
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- PLAN DETAILS
- EROSION CONTROL PLAN
- SIGNING & MARKING PLAN
- TRAFFIC CONTROL
- DETOUR
- ALIGNMENT DIAGRAM



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 EMAIL: kristina.betzold@wisconsin.gov

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 WEST ALLIS, WI 53214
 ATTN: GREG BOERNER
 MOBILE: (608) 409-5861
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GAS

* WE ENERGIES - GAS
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* DENOTES MEMBER OF DIGGERS HOTLINE

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EROSION CONTROL NOTES

EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES AND THE WDNR CONSERVATION PRACTICE TECHNICAL STANDARDS.

EROSION CONTROL ITEMS SHOWN ARE APPROXIMATE, THE EXACT LOCATION SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL SUCH TIME AS THE ENGINEER DETERMINES THAT THE MEASURE IS NO LONGER NECESSARY. CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING EROSION CONTROL MEASURE AS DIRECTED BY THE ENGINEER.

SILT FENCE SHALL BE INSTALLED IN AREAS WHERE ON-SITE SOILS AND STORMWATER MAY EXIT THE CONSTRUCTION SITE.

TRACKING PAD(S) SHALL BE MAINTAINED AND ALL ACCESS TO AND FROM RECONSTRUCT AREAS SHALL BE VIA TRACKING PAD(S) ONLY.

GEOTEXTILE FABRIC SHALL BE PLACED UNDER AREAS REQUIRING RIPRAP.

INSTALL CULVERT PIPE DITCH CHECKS IN ALL UPSTREAM ENDS OF CULVERTS WITH APRON ENDWALLS.

ALL OFF-SITE SEDIMENT DEPOSITS OCCURRING AS A RESULT OF CONSTRUCTION WORK OR A STORM EVENT SHALL BE CLEANED UP BY THE END OF EACH DAY. FLUSHING SHALL NOT BE ALLOWED.

FOR ANY DISTURBED AREA THAT REMAINS INACTIVE FOR MORE THAN 7 WORKING DAYS, OR WHERE GRADING EXTENDS BEYOND THE PERMANENT SEEDING DEADLINES, THE SITE MUST BE TREATED WITH TEMPORARY STABILIZATION MEASURES SUCH AS SOIL TREATMENT, TEMPORARY SEEDING, AND/OR MULCHING.

ALL DISTURBED AREAS SHALL BE TREATED WITH STABILIZATION MEASURES AS SPECIFIED WITHIN 3 WORKING DAYS OF FINAL GRADING.

ANY SOIL EROSION THAT OCCURS AFTER FINAL GRADING AND/OR THE APPLICATION OF STABILIZATION MEASURES MUST BE REPAIRED AND THE STABILIZATION WORK REDONE.

CONSTRUCTION SEQUENCE

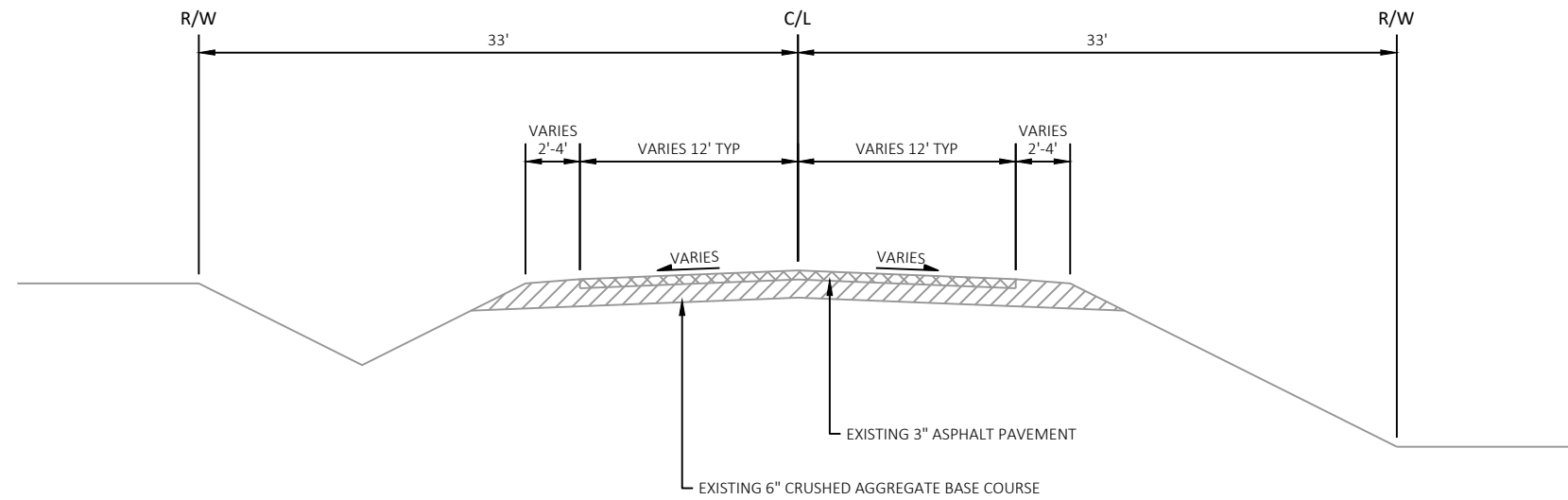
1. IDENTIFY AND MARK WETLAND BOUNDARIES TO AVOID UNINTENDED IMPACTS. TEMPORARY CROSSING OF THE WETLANDS IS NOT ALLOWED WITHOUT PRIOR COORDINATION AND ACCEPTANCE FROM WDNR. ANY APPROVED WETLAND CROSSING SHALL SPECIFY THE LOCATION AND THE MEANS/METHODS PROPOSED TO LIMIT DISTURBANCE.
2. COMPLETE SLOPE STAKING TO AVOID EXCESSIVE CONSTRUCTION DISTURBANCE.
3. INSTALL TEMPORARY EROSION CONTROL MEASURES SUCH AS SILT FENCE, SEDIMENT TRAPS, DITCH CHECKS, PIPE CHECKS, AND TRACKING PAD(S). ADD TEMPORARY EROSION CONTROL MEASURES AS GRADING WORK PROGRESSES. TOPSOIL STRIPPING SHALL NOT BEGIN UNTIL EROSION CONTROL MEASURES ARE IN PLACE.
4. STRIP TOPSOIL AND CLEAR & GRUB IN A PROGRESSIVE MANNER THROUGHOUT THE PROJECT, AS NEEDED FOR GRADING WORK. CONSTRUCT PERIMETER CONTROL AROUND ANY STOCKPILES AND PLACE TEMPORARY SEEDING AS REQUIRED. LIMIT THE SIZE OF DISTURBED AREAS TO THAT WHICH CAN BE READILY STABILIZED.
5. COMPLETE GRADING WORK, PLACING PERMANENT EROSION CONTROL MEASURES AS PRACTICAL. CONSTRUCT ROADWAY AND ADJUST TEMPORARY EROSION CONTROL MEASURES AS NECESSARY.
6. COMPLETE REMAINING TOPSOIL, LANDSCAPING, AND PERMANENT EROSION CONTROL MEASURES THROUGHOUT THE PROJECT. REMOVE ACCUMULATED SEDIMENT FROM TEMPORARY EROSION CONTROL MEASURES AND REMOVE AND DISPOSE OF USED EROSION CONTROL DEVICES AFTER 70% VEGETATIVE STABILIZATION HAS OCCURRED.



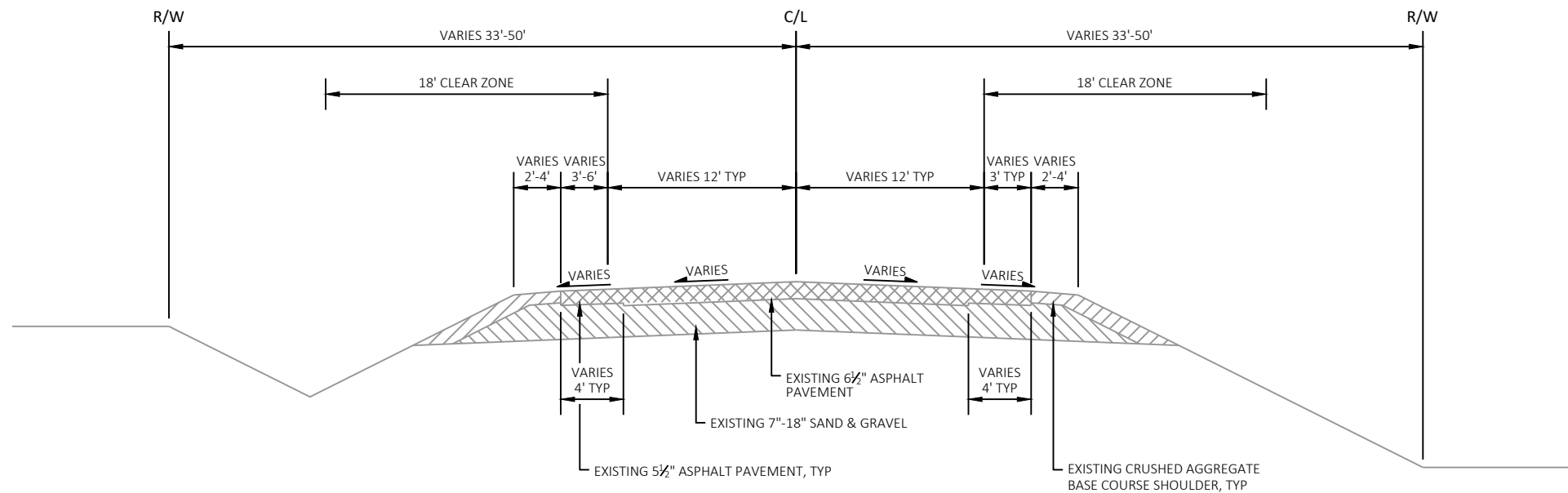
| CONTROL POINT TABLE | | | |
|---------------------|-------------|-------------|-----------|
| POINT # | NORTHING | EASTING | ELEVATION |
| 10 | 170083.4950 | 318555.6700 | 1079.61 |
| 11 | 170138.4040 | 317955.6880 | 1077.66 |
| 10607 | 170140.6740 | 318046.4500 | 1077.75 |
| 10618 | 170161.8510 | 317635.9010 | 1071.14 |
| 10737 | 169933.1390 | 317739.6820 | 1074.38 |
| 30000 | 170755.8020 | 317556.4480 | 1054.12 |
| 30001 | 171024.9700 | 317092.6340 | 1050.00 |

| BENCHMARKS | | |
|-------------|--|-----------|
| BM | DESCRIPTION | ELEVATION |
| ADDISON GPS | NGS BRASS CAP IN CONCRETE, WEST SIDE OF FORK ROAD, 200' NORTH OF CTH S | 1083.20 |

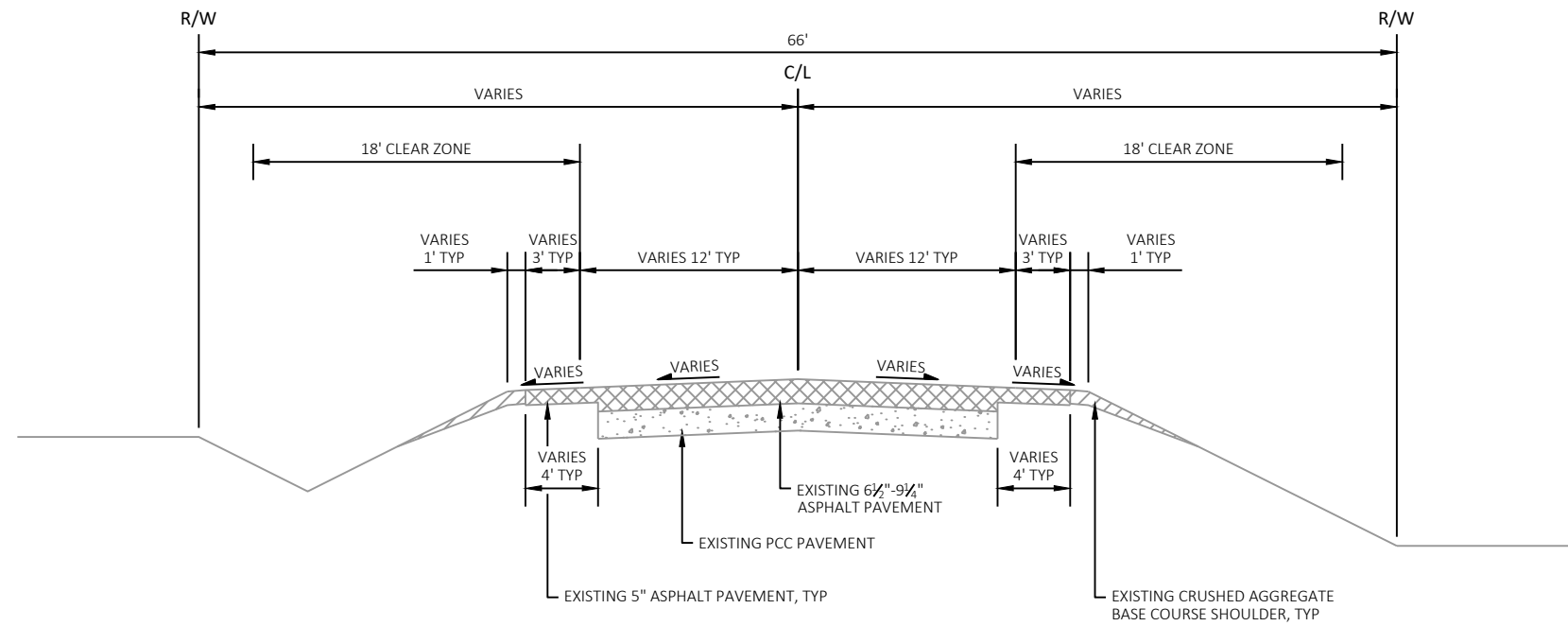
**VERTICAL DATUM REFERENCED TO NAVD88 (2012).



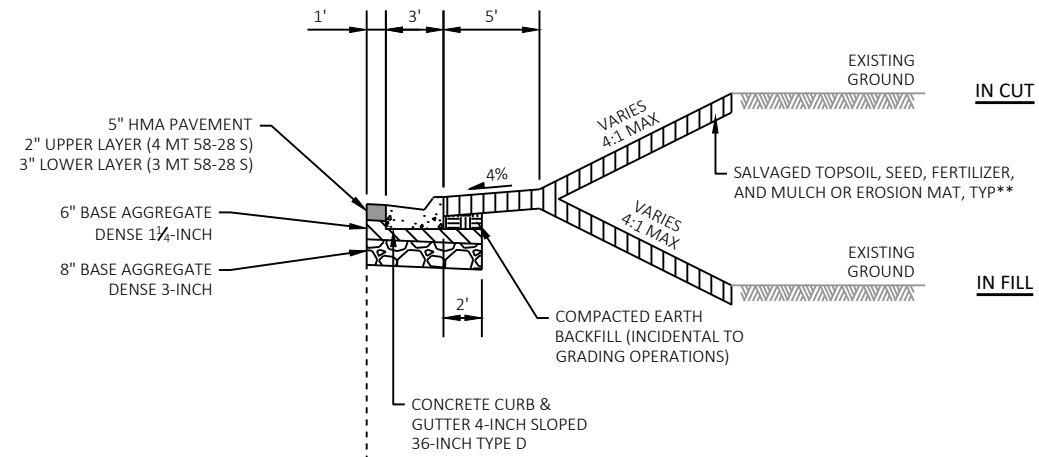
TYPICAL EXISTING SECTION
 CTH S
 STA 197+00 - STA 214+60



TYPICAL EXISTING SECTION
 CTH R
 STA 244+50 - STA 255+19



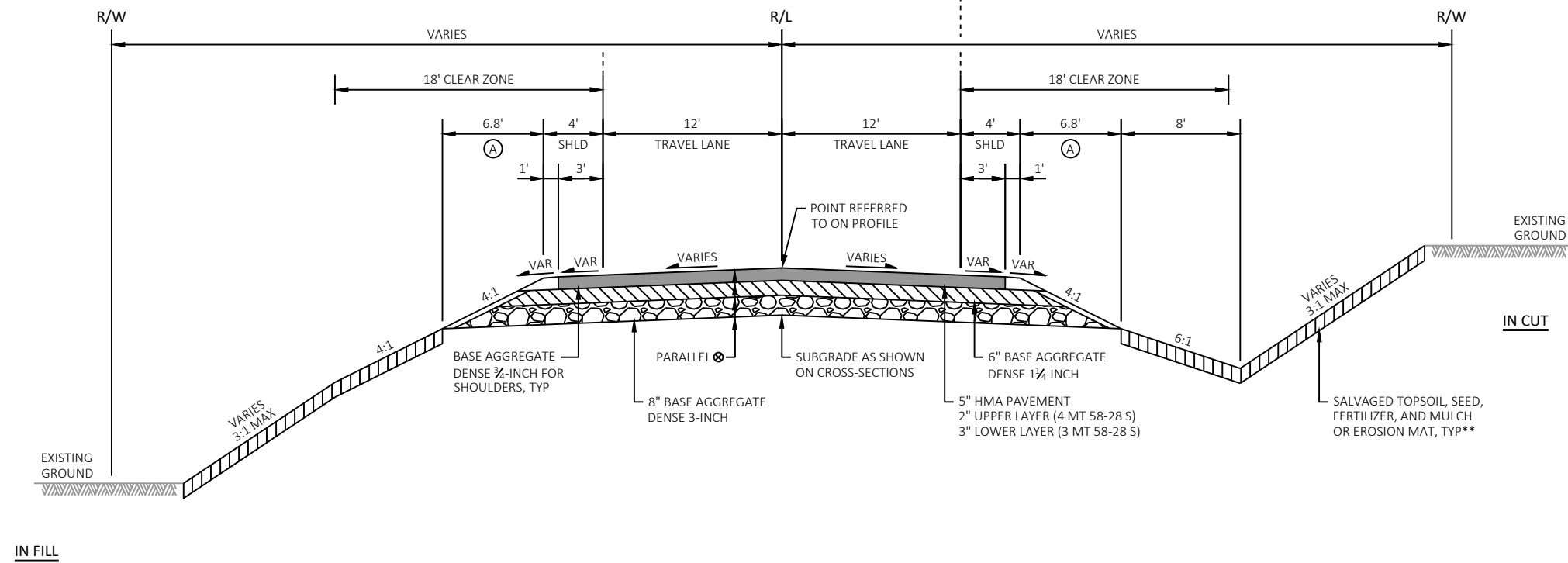
TYPICAL EXISTING SECTION
 STH 175
 STA 816+85 - STA 825+73



CURB & GUTTER SECTION

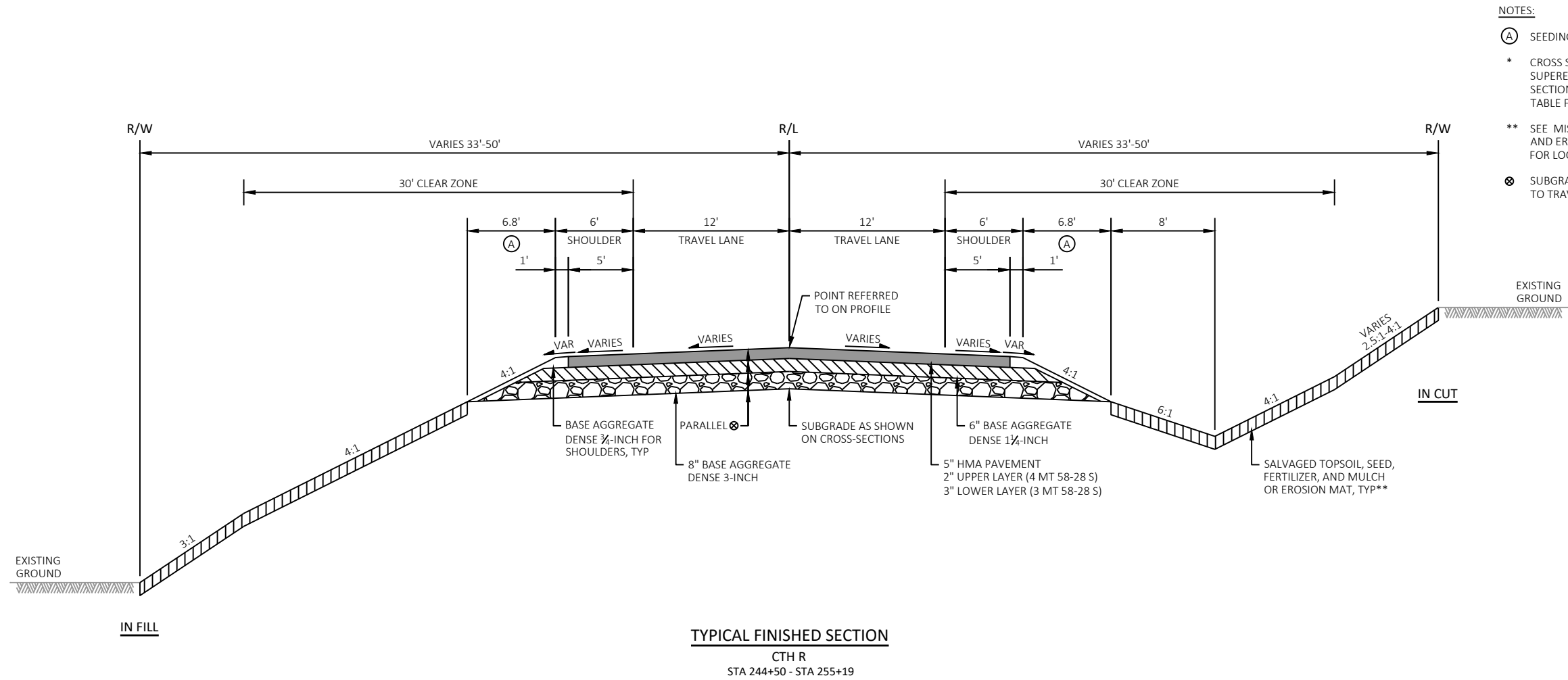
NOTES:

- (A) SEEDING & FERTILIZER
- * CROSS SLOPE VARIES DUE TO SUPERELEVATION (SEE CROSS SECTIONS AND SUPERELEVATION TABLE FOR FURTHER DETAILS).
- ** SEE MISCELLANEOUS QUANTITIES AND EROSION CONTROL PLANS FOR LOCATIONS AND TYPES.
- ⊗ SUBGRADE SLOPES ARE PARALLEL TO TRAVEL LANE.



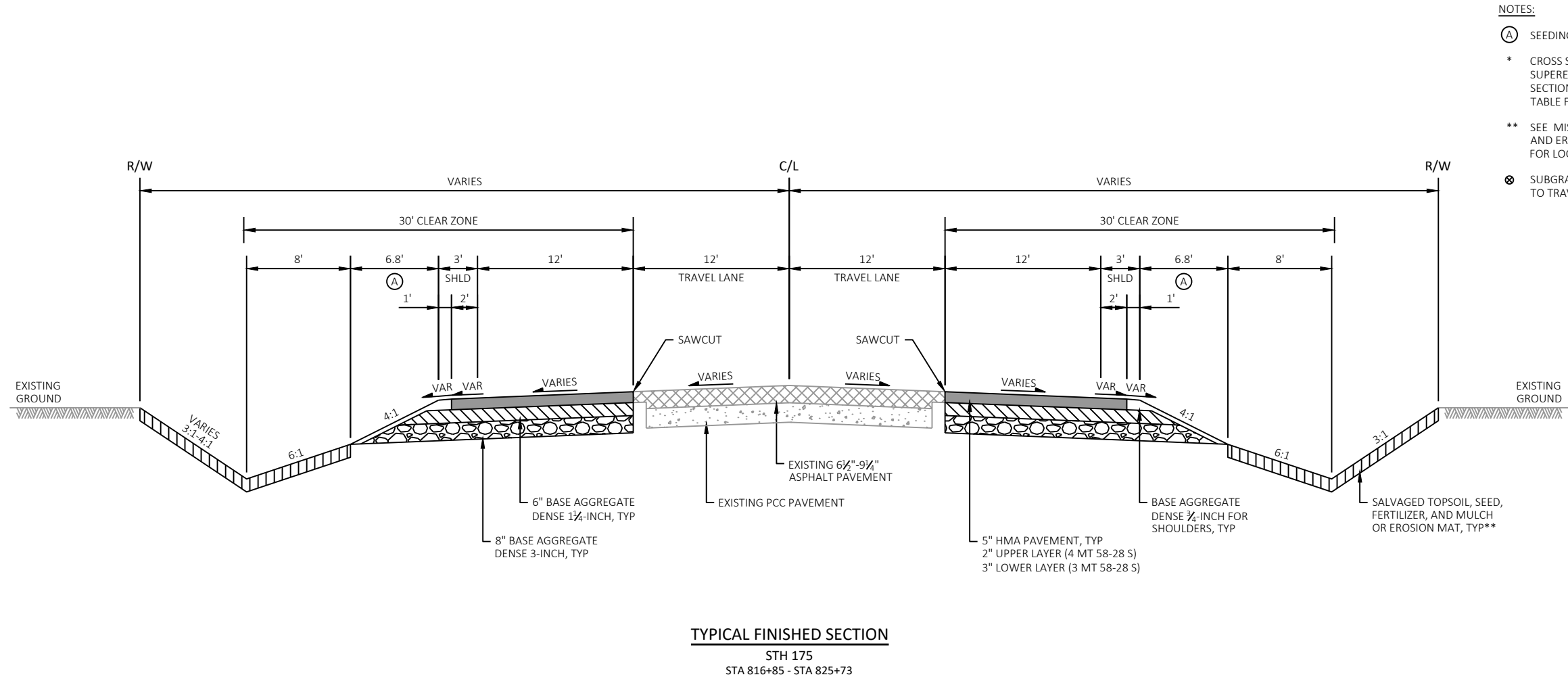
TYPICAL FINISHED SECTION

CTH 5
STA 197+00 - STA 214+60



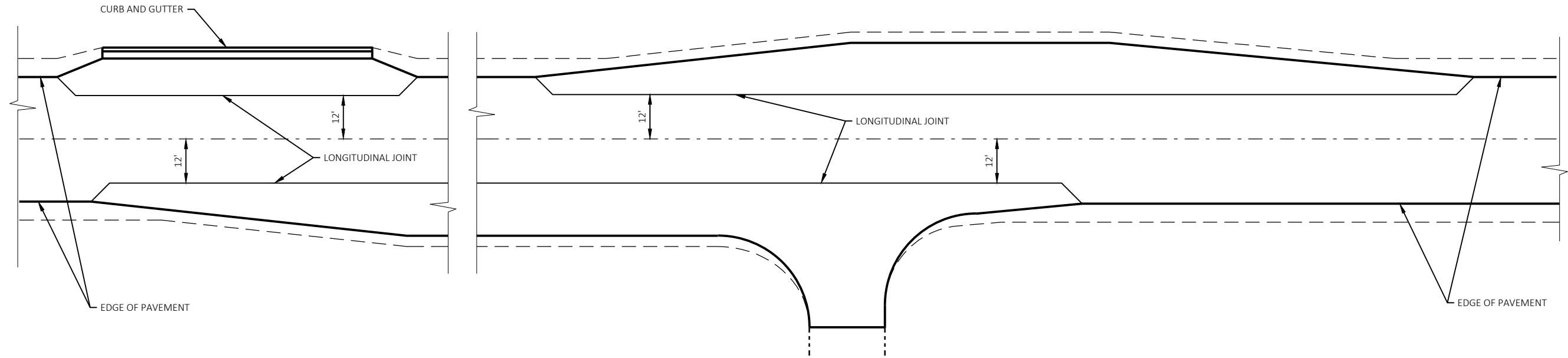
NOTES:

- (A) SEEDING & FERTILIZER
- * CROSS SLOPE VARIES DUE TO SUPERELEVATION (SEE CROSS SECTIONS AND SUPERELEVATION TABLE FOR FURTHER DETAILS).
- ** SEE MISCELLANEOUS QUANTITIES AND EROSION CONTROL PLANS FOR LOCATIONS AND TYPES.
- ⊗ SUBGRADE SLOPES ARE PARALLEL TO TRAVEL LANE.

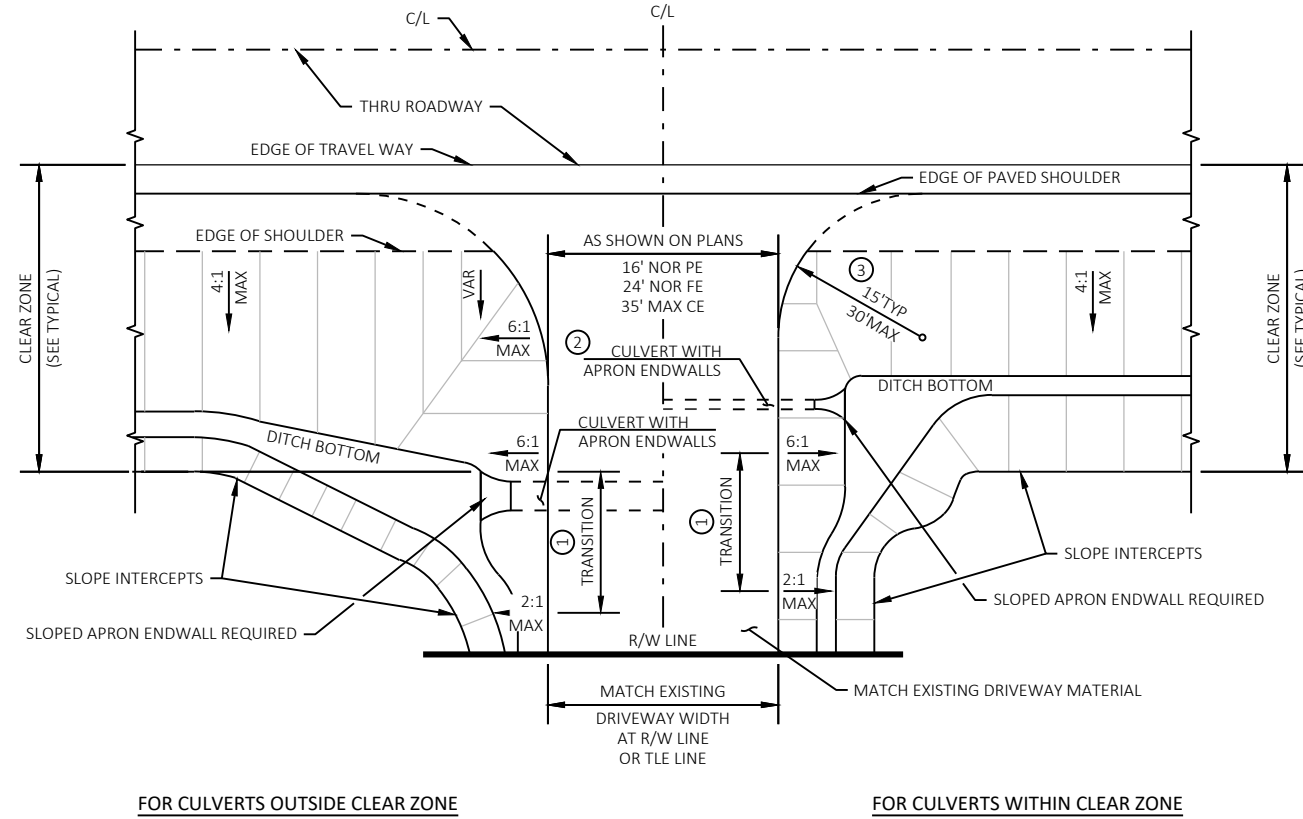


- NOTES:**
- (A) SEEDING & FERTILIZER
 - * CROSS SLOPE VARIES DUE TO SUPERELEVATION (SEE CROSS SECTIONS AND SUPERELEVATION TABLE FOR FURTHER DETAILS).
 - ** SEE MISCELLANEOUS QUANTITIES AND EROSION CONTROL PLANS FOR LOCATIONS AND TYPES.
 - ⊗ SUBGRADE SLOPES ARE PARALLEL TO TRAVEL LANE.

* TO BE USED AT ALL INTERSECTIONS, BYPASS LANES, PASSING LANES, AND RURAL CURB SECTIONS



HMA LONGITUDINAL JOINT DETAIL

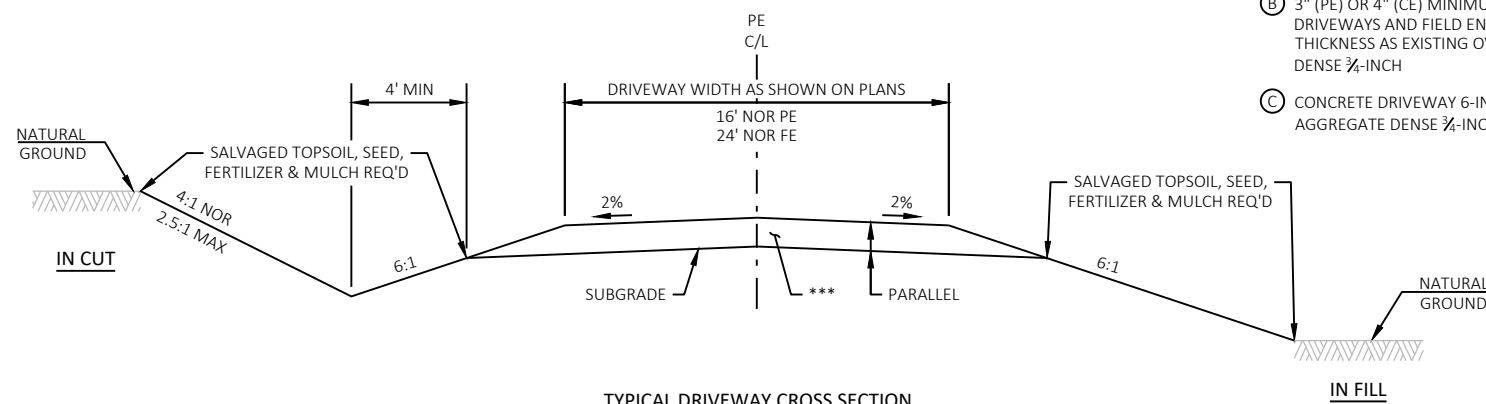


- ① TRANSITION TO BE ACCOMPLISHED WITHIN THE RIGHT OF WAY
- ② BLEND 6 : 1 SLOPES TO MATCH APRON ENDWALLS
- ③ USE LARGER PAVING RADIUS FOR PAVING IN HIGHER SPEED ZONES (> 40 MPH)

FOR CULVERTS OUTSIDE CLEAR ZONE

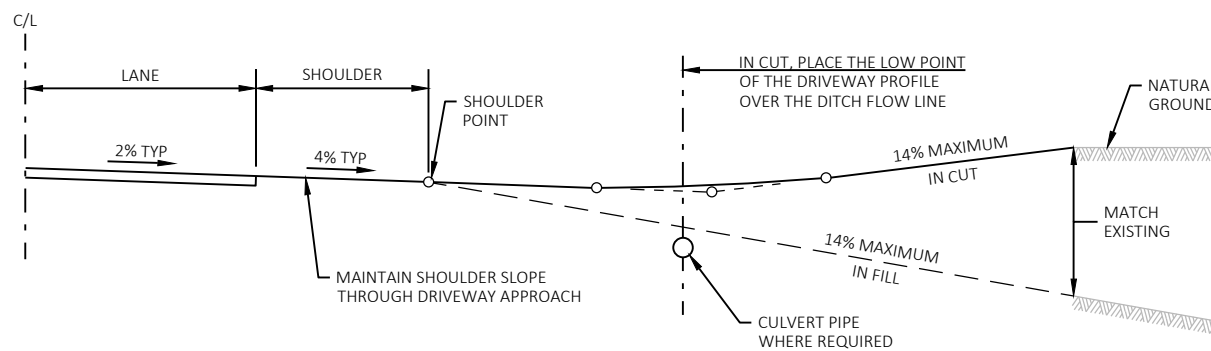
FOR CULVERTS WITHIN CLEAR ZONE

RURAL DRIVEWAY PLAN VIEW



TYPICAL DRIVEWAY CROSS SECTION

- *** (A) 6" BASE AGGREGATE DENSE 3/4-INCH
- (B) 3" (PE) OR 4" (CE) MINIMUM ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES OR SAME THICKNESS AS EXISTING OVER 6" BASE AGGREGATE DENSE 3/4-INCH
- (C) CONCRETE DRIVEWAY 6-INCH OVER 6" BASE AGGREGATE DENSE 3/4-INCH

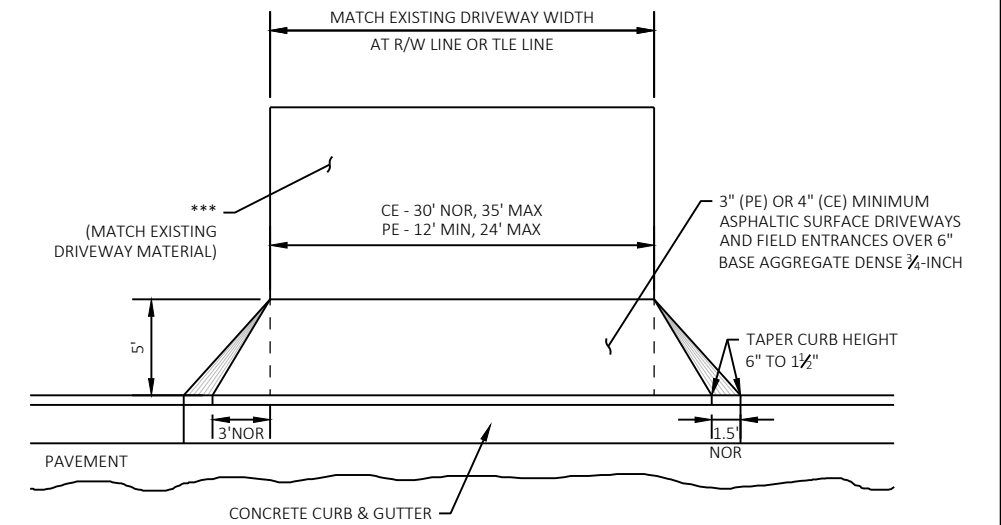


TYPICAL DRIVEWAY PROFILE

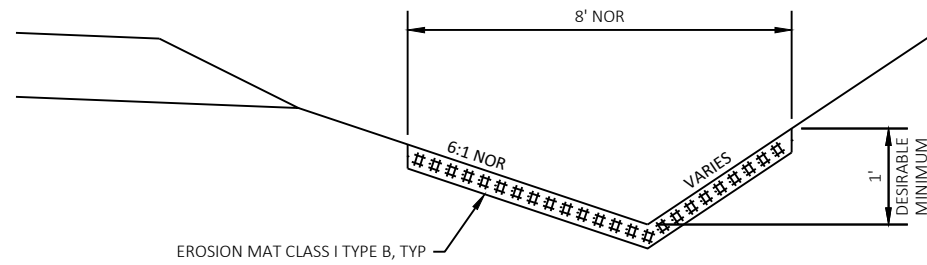
RURAL ENTRANCE DETAIL

*** 6" BASE AGGREGATE DENSE 3/4-INCH
 3" (PE) OR 4" (CE) MINIMUM ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES OR SAME THICKNESS AS EXISTING OVER 6" BASE AGGREGATE DENSE 3/4-INCH
 CONCRETE DRIVEWAY 6-INCH OVER 6" BASE AGGREGATE DENSE 3/4-INCH

NOTE: ALGEBRAIC DIFFERENCE BETWEEN TANGENT GRADES G1 & G2 TO NOT EXCEED 15%



ENTRANCE DETAIL WITH CURB & GUTTER



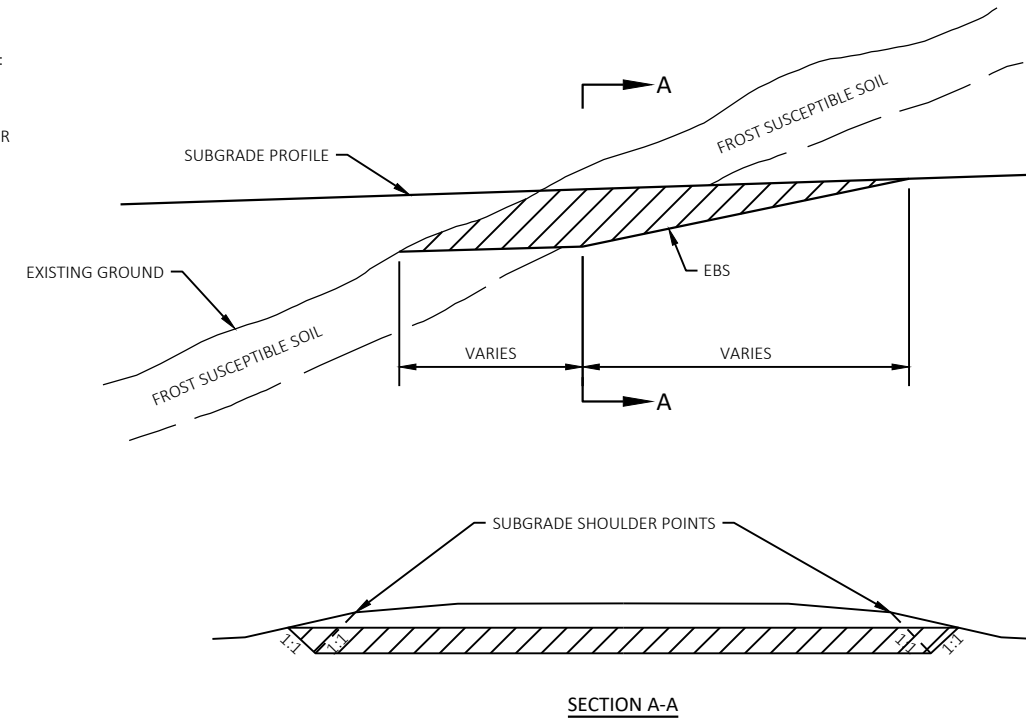
EROSION MAT DETAIL FOR DITCHES

SCHEDULE OF EXCAVATION BELOW SUBGRADE CONSTRUCTION OPERATIONS:

1. EXCAVATE TO SUBGRADE ELEVATIONS SHOWN.
2. PROOF ROLL SUBGRADE.
3. IF ENGINEER DEEMS SUBGRADE ACCEPTABLE, CONSTRUCT ROADWAY PER TYPICAL SECTIONS.
4. IF ENGINEER DEEMS SUBGRADE UNACCEPTABLE, EBS PER ENGINEER'S RECOMMENDATION.
5. IF ENGINEER DEEMS EBS SUBGRADE ACCEPTABLE, BACKFILL WITH BREAKER RUN AND CONSTRUCT ROADWAY PER TYPICAL SECTIONS.
6. IF ENGINEER DEEMS EBS SUBGRADE UNACCEPTABLE, ADDITIONAL CONSTRUCTION OPERATIONS SHALL BE COORDINATED WITH THE ENGINEER AND MAY INCLUDE THE USE OF GEOGRID TYPE SR.

NOTES:

1. EBS SHALL BE PAID FOR AS EXCAVATION BELOW SUBGRADE - 205.0010.02.
2. EXACT LOCATIONS AND EXTENT OF EBS SECTIONS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
3. FILL VOID WITH BREAKER RUN.
4. BACKFILL MUST BE HOMOGENEOUS WITH ADJOINING FILL MATERIAL.
5. THE FILL SECTION WITHIN 100' OF THE MOUNT OF THE CUT SHALL BE KEPT 2' BELOW SUBGRADE UNTIL EBS IS COMPLETED.

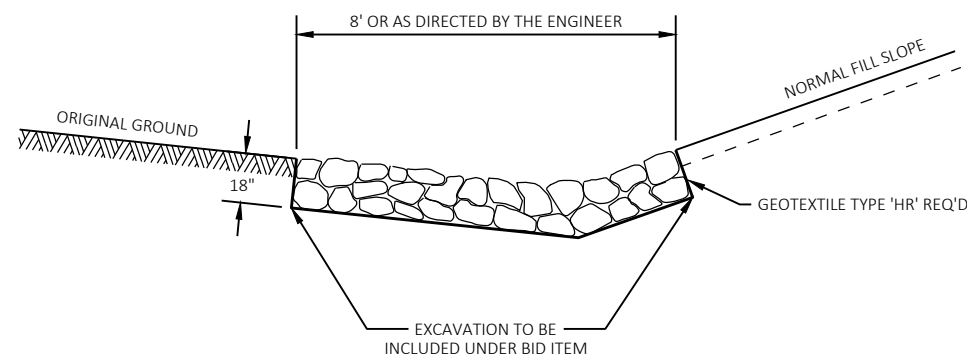


EXCAVATION BELOW SUBGRADE

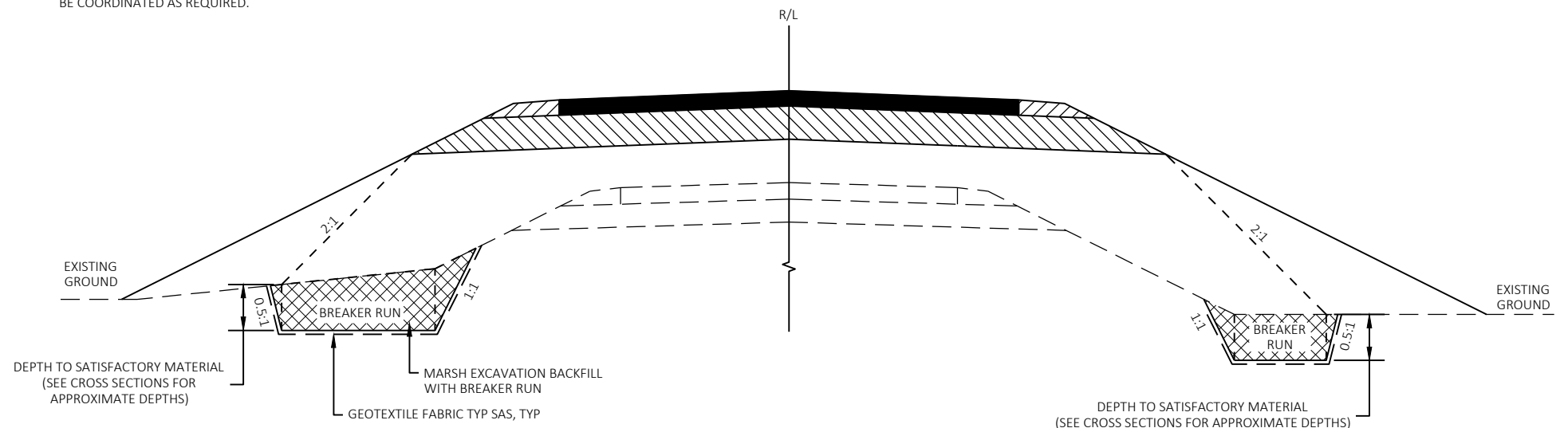
SCHEDULE OF PARTIAL MARSH EXCAVATION CONSTRUCTION OPERATIONS:

1. EXCAVATE TO MARSH EXCAVATION ELEVATIONS SHOWN.
2. IF ENGINEER DEEMS SUBGRADE ACCEPTABLE, PLACE GEOTEXTILE FABRIC TYPE SAS AND BACKFILL WITH BREAKER RUN TO EXISTING GROUND. REMAINDER OF VOID TO BE BACKFILLED WITH FILL TO SUBGRADE. CONSTRUCT ROADWAY PER TYPICAL SECTIONS. ADDITIONAL CONSTRUCTION OPERATIONS FOR MARSH EXCAVATION DEEMED UNACCEPTABLE WILL BE COORDINATED AS REQUIRED.

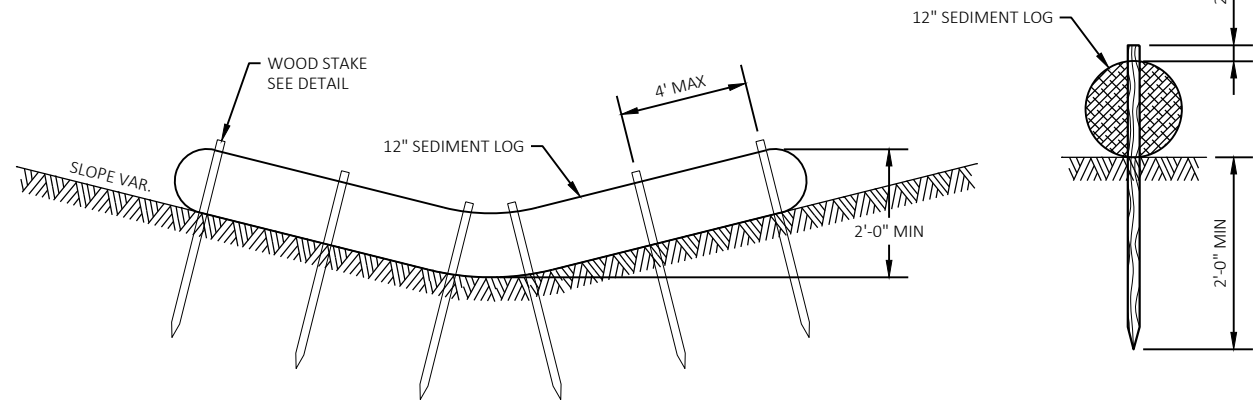
☒ MARSH EXCAVATION



DETAIL FOR RIPRAP MEDIUM IN DITCHES



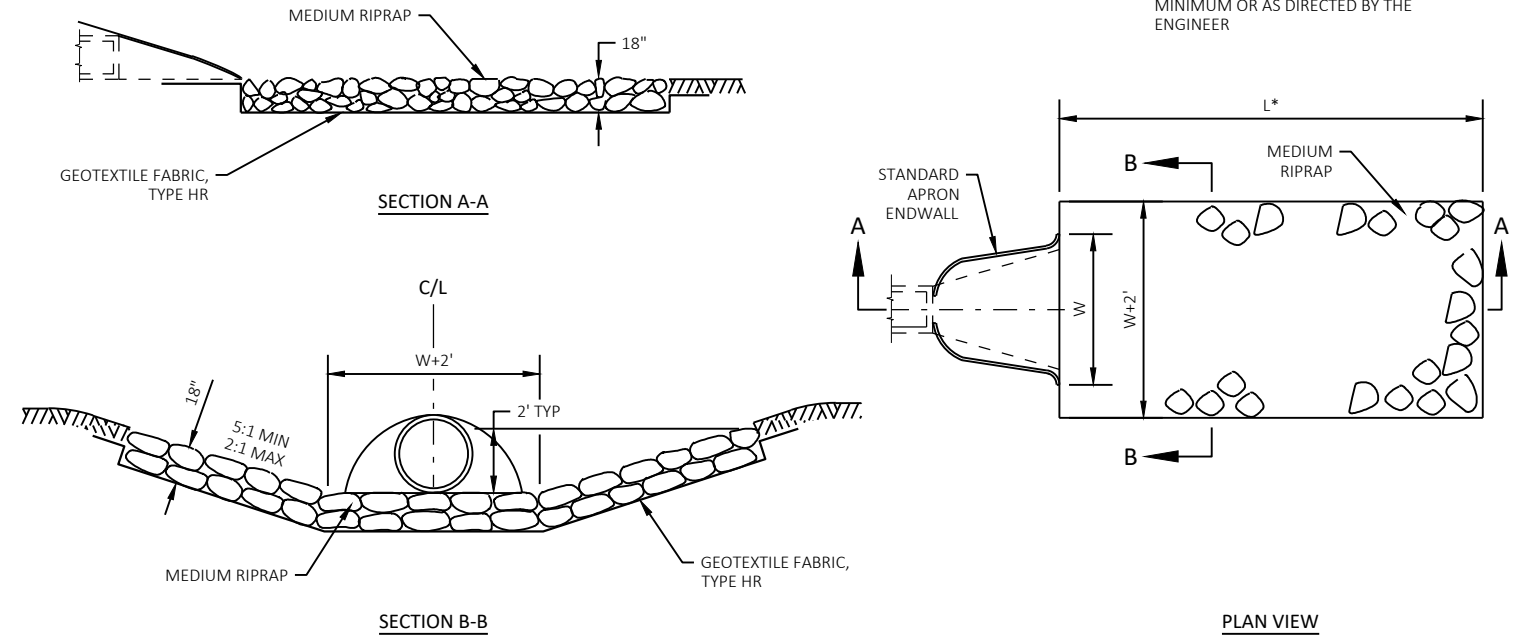
PARTIAL MARSH EXCAVATION DETAIL



SECTION VIEW

WOOD STAKE DETAIL

SEDIMENT LOG DETAIL

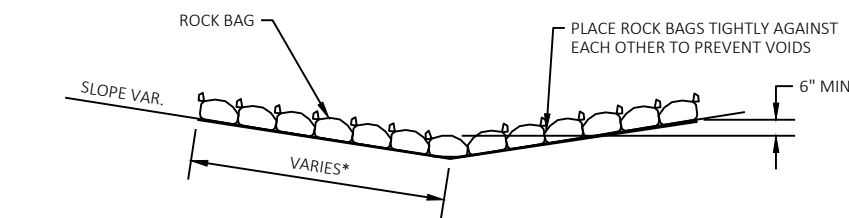


SECTION A-A

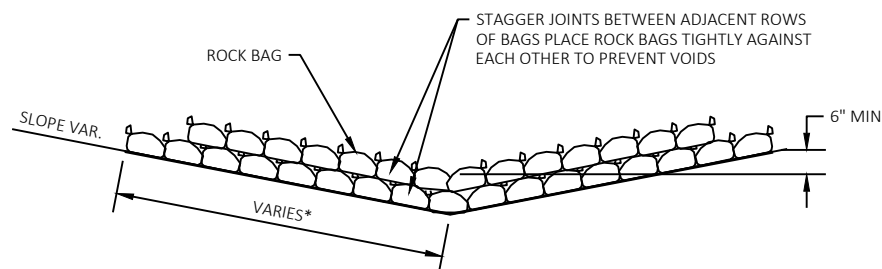
SECTION B-B

PLAN VIEW

MEDIUM RIPRAP AND GEOTEXTILE FABRIC
 DETAIL AT APRON ENDWALLS



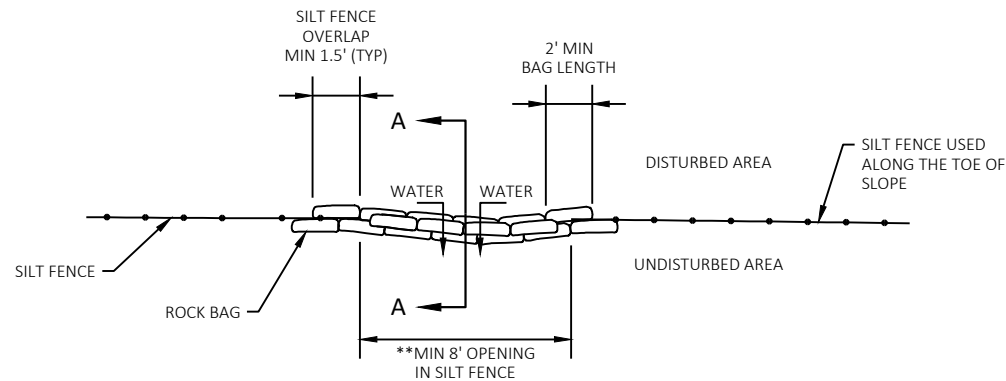
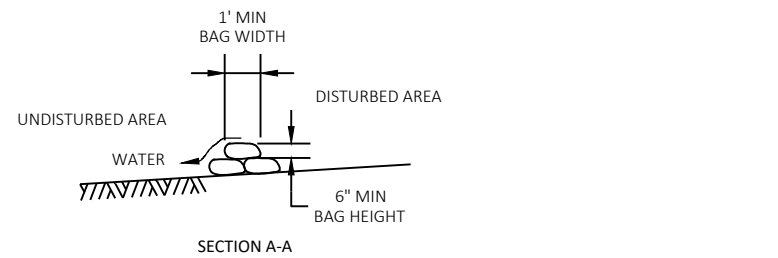
SIDE VIEW (SINGLE LAYER)



SIDE VIEW (MULTIPLE LAYER)

* LENGTH AND NUMBER OF BAGS MAY VARY DEPENDING ON DESIRED DEPTH OF WATER POOL

ROCK BAGS USED FOR DITCH CHECKS

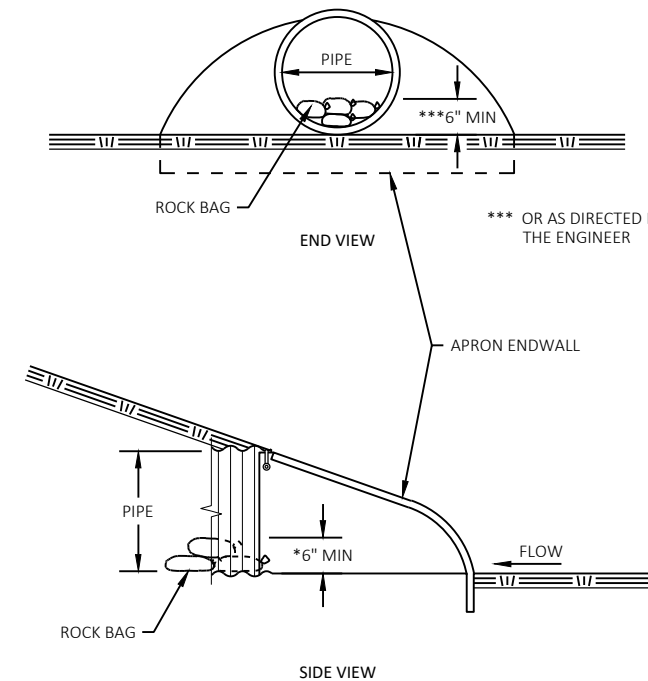


** 17 ROCK BAGS MINIMUM PER 8' OPENING.

PLAN VIEW

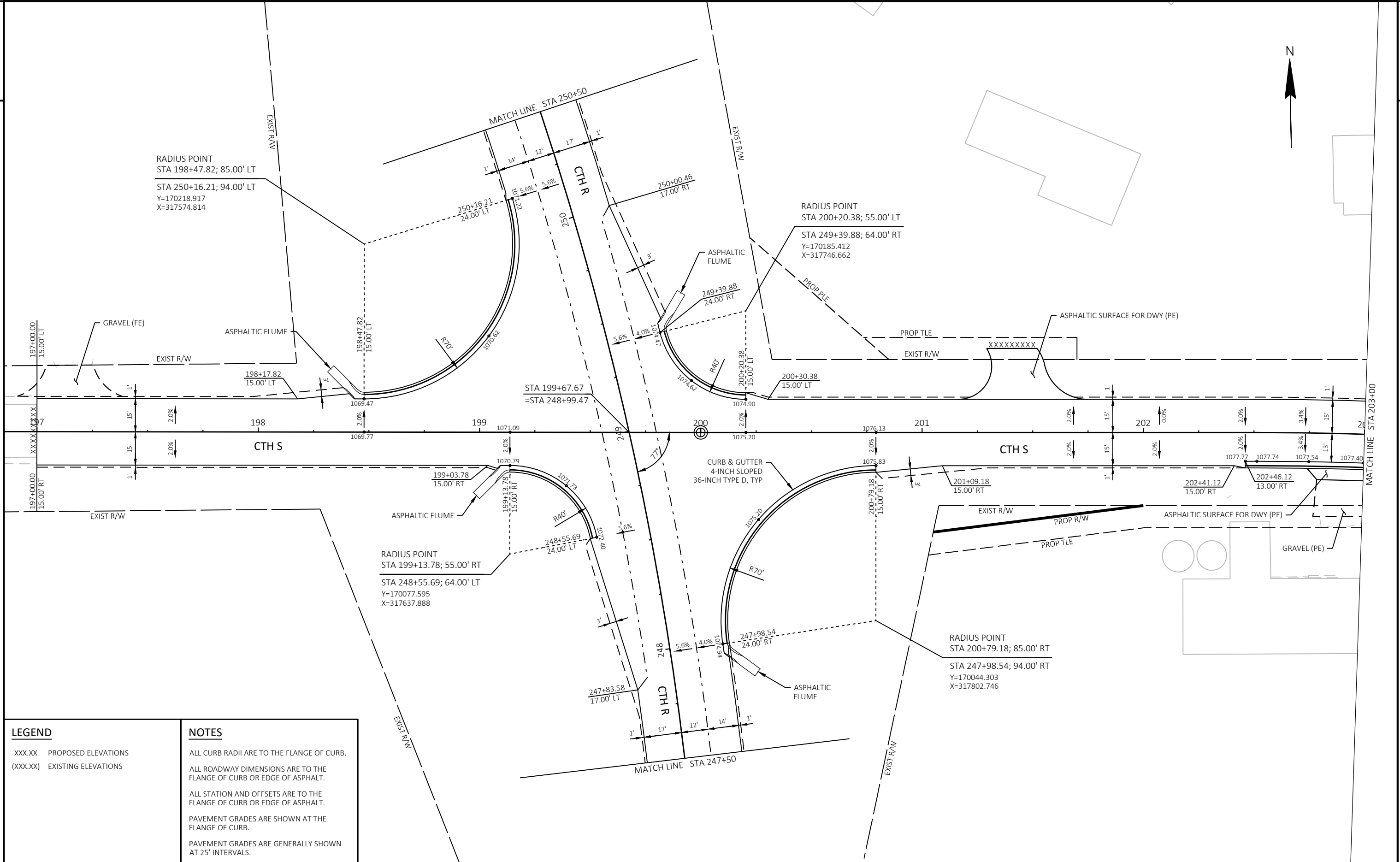
ROCK BAGS USED FOR SILT FENCE RELIEF POINT

ROCK BAGS DETAIL

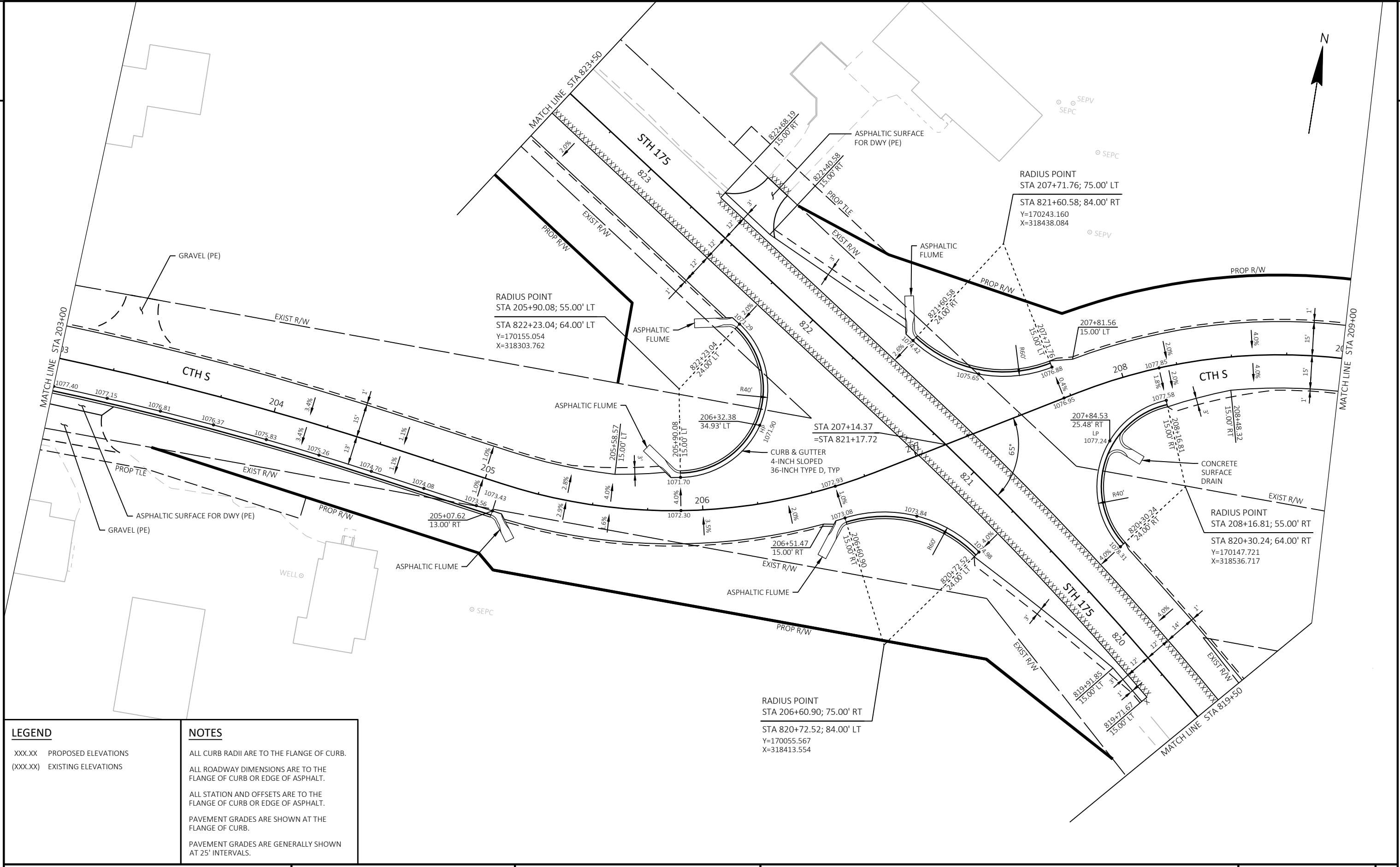


ROCK BAGS USED FOR CULVERT PIPE CHECKS

| ESTIMATED BAG SIZE = 18" X 12" X 6" | |
|-------------------------------------|--------------------------|
| PIPE SIZE | ESTIMATED NUMBER OF BAGS |
| 12" | 1 |
| 15" | 2 |
| 18" | 2 |
| 21" | 3 |
| 24" | 3 |
| 30" | 5 |
| 36" | 7 |
| 42" | 7 |
| 48" | 10 |
| 54" | 10 |
| 60" | 13 |
| 66" | 14 |
| 72" | 16 |
| 14"X23" | 3 |
| 19"X30" | 5 |
| 24"X38" | 8 |
| 29"X45" | 10 |
| 34"X53" | 10 |
| 38"X60" | 13 |
| 48"X76" | 18 |



| LEGEND | NOTES |
|------------------------------|---|
| XXX.XX PROPOSED ELEVATIONS | ALL CURB RADII ARE TO THE FLANGE OF CURB. |
| (XXX.XX) EXISTING ELEVATIONS | ALL ROADWAY DIMENSIONS ARE TO THE FLANGE OF CURB OR EDGE OF ASPHALT. |
| | ALL STATION AND OFFSETS ARE TO THE FLANGE OF CURB OR EDGE OF ASPHALT. |
| | PAVEMENT GRADES ARE SHOWN AT THE FLANGE OF CURB. |
| | PAVEMENT GRADES ARE GENERALLY SHOWN AT 25' INTERVALS. |



LEGEND

XXX.XX PROPOSED ELEVATIONS
 (XXX.XX) EXISTING ELEVATIONS

NOTES

ALL CURB RADII ARE TO THE FLANGE OF CURB.
 ALL ROADWAY DIMENSIONS ARE TO THE FLANGE OF CURB OR EDGE OF ASPHALT.
 ALL STATION AND OFFSETS ARE TO THE FLANGE OF CURB OR EDGE OF ASPHALT.
 PAVEMENT GRADES ARE SHOWN AT THE FLANGE OF CURB.
 PAVEMENT GRADES ARE GENERALLY SHOWN AT 25' INTERVALS.

PROJECT NO: HWY24-02

HWY: CTH S

COUNTY: WASHINGTON

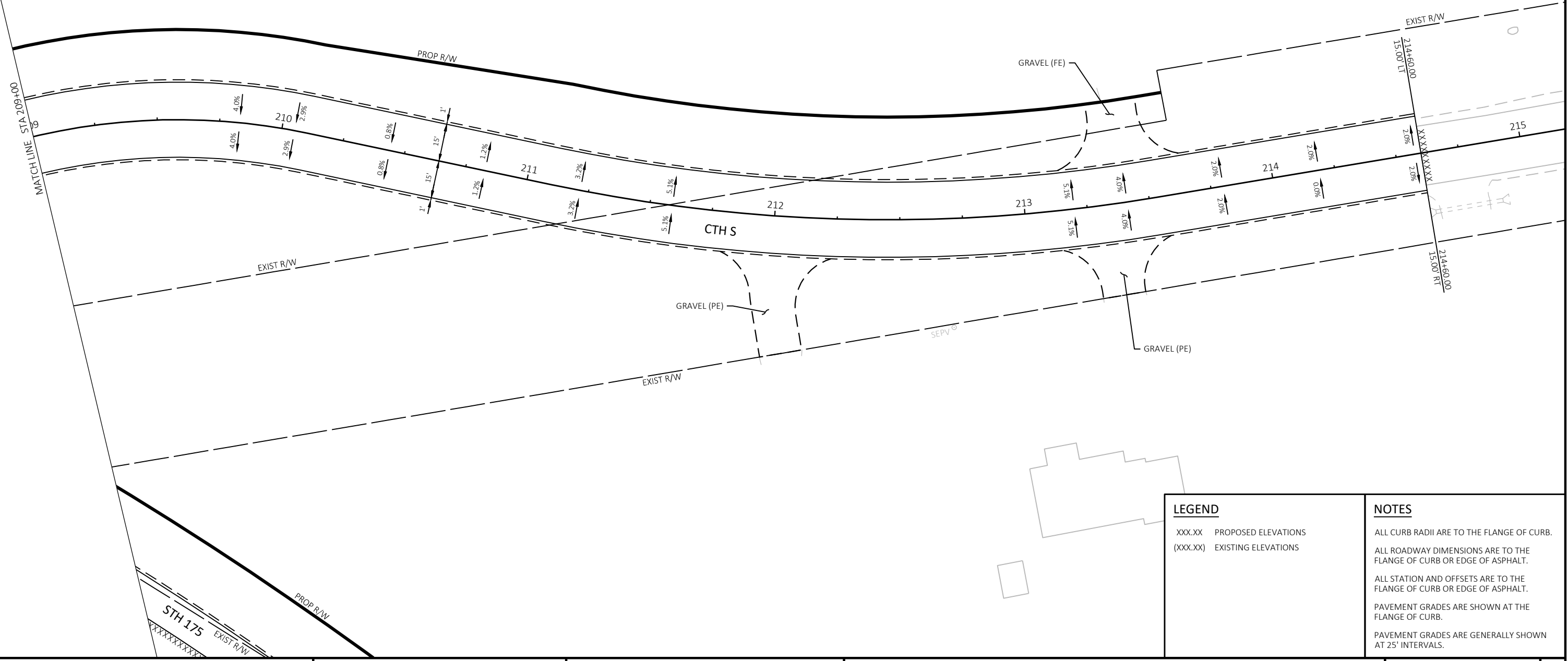
PLAN DETAILS - CTH S

SHEET

E



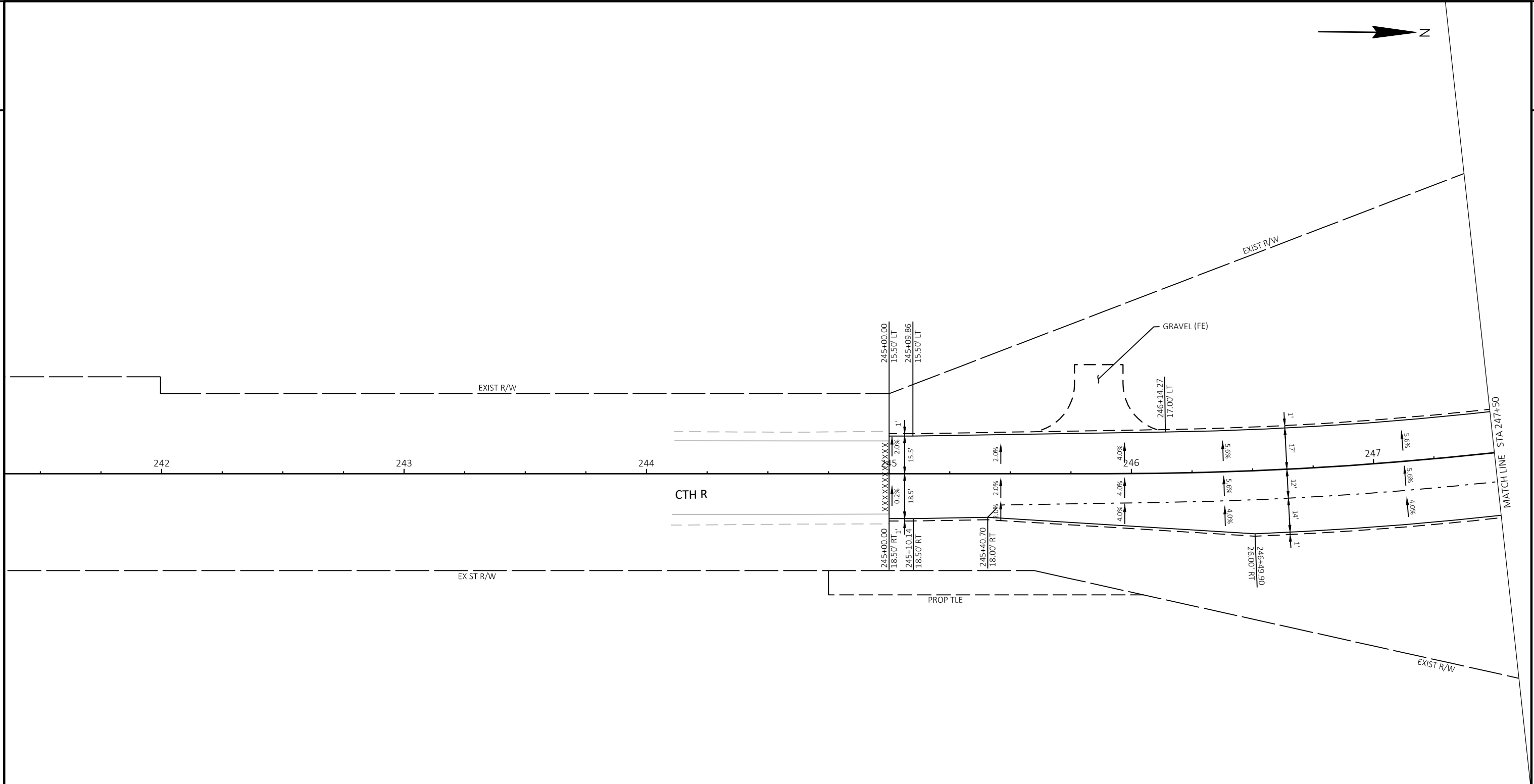
MATCH LINE STA 209+00



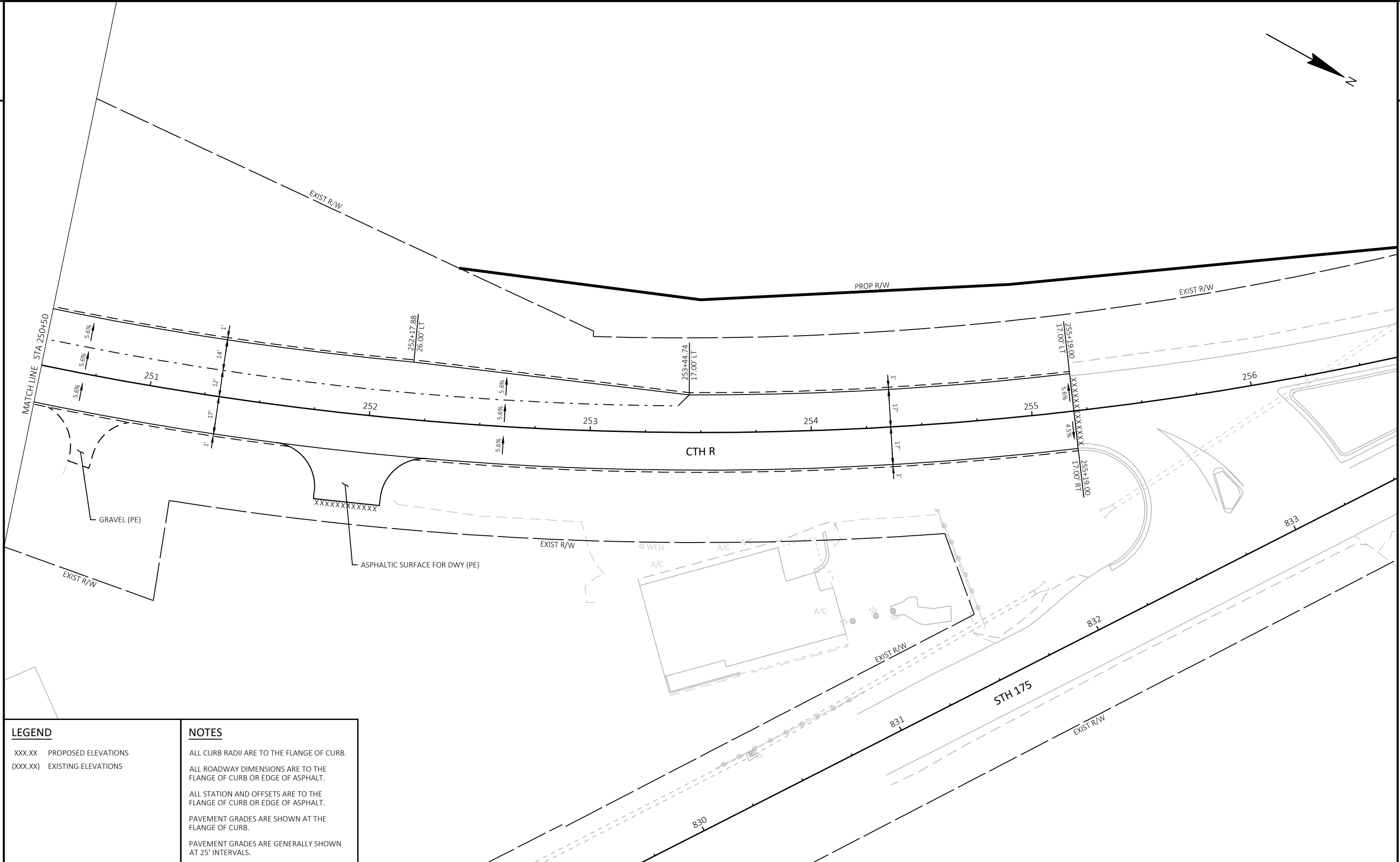
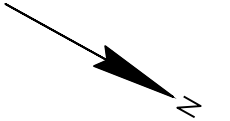
| LEGEND | |
|----------|---------------------|
| XXX.XX | PROPOSED ELEVATIONS |
| (XXX.XX) | EXISTING ELEVATIONS |

| NOTES |
|---|
| ALL CURB RADII ARE TO THE FLANGE OF CURB. |
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| | | | | | |
|----------------------|------------|--------------------|----------------------|-------|----------|
| PROJECT NO: HWY24-02 | HWY: CTH S | COUNTY: WASHINGTON | PLAN DETAILS - CTH S | SHEET | E |
|----------------------|------------|--------------------|----------------------|-------|----------|



| LEGEND | NOTES |
|------------------------------|---|
| XXX.XX PROPOSED ELEVATIONS | ALL CURB RADII ARE TO THE FLANGE OF CURB. |
| (XXX.XX) EXISTING ELEVATIONS | ALL ROADWAY DIMENSIONS ARE TO THE FLANGE OF CURB OR EDGE OF ASPHALT. |
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| LEGEND | NOTES |
|------------------------------|---|
| XXX.XX PROPOSED ELEVATIONS | ALL CURB RADII ARE TO THE FLANGE OF CURB. |
| (XXX.XX) EXISTING ELEVATIONS | ALL ROADWAY DIMENSIONS ARE TO THE FLANGE OF CURB OR EDGE OF ASPHALT. |
| | ALL STATION AND OFFSETS ARE TO THE FLANGE OF CURB OR EDGE OF ASPHALT. |
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2

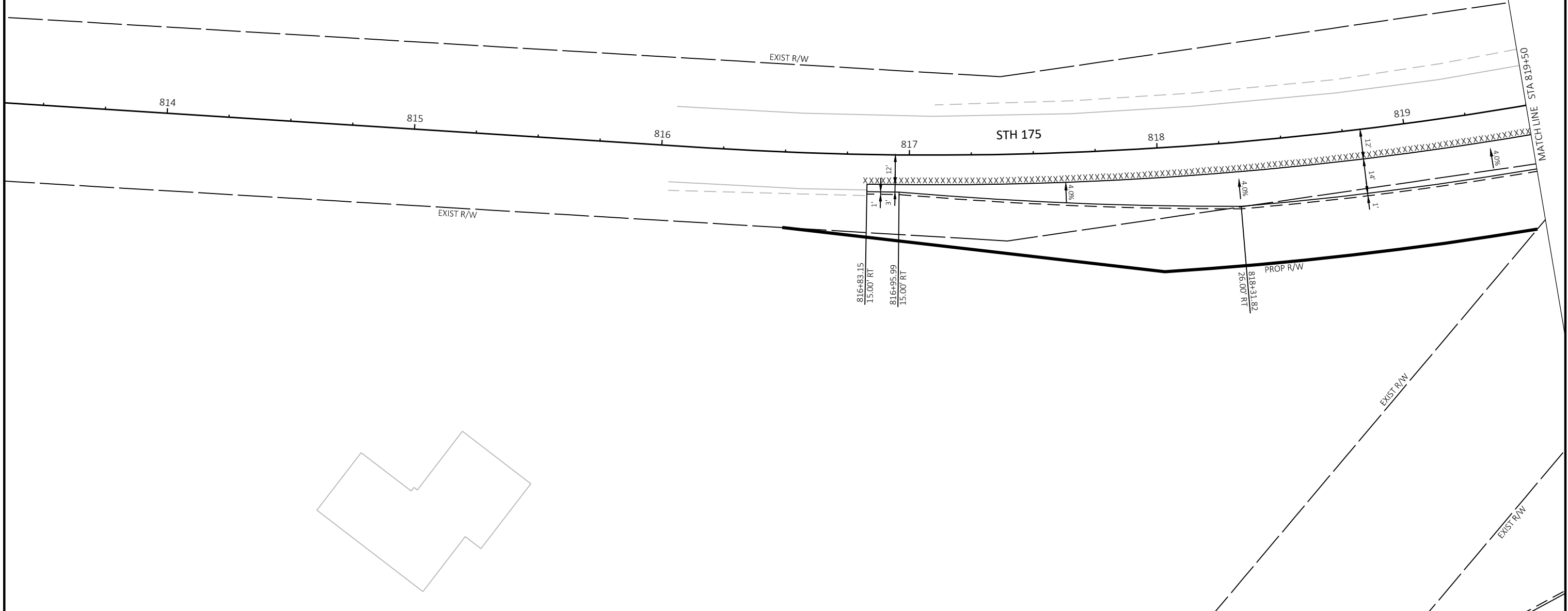
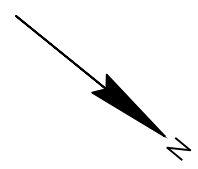
LEGEND

XXX.XX PROPOSED ELEVATIONS
(XXX.XX) EXISTING ELEVATIONS

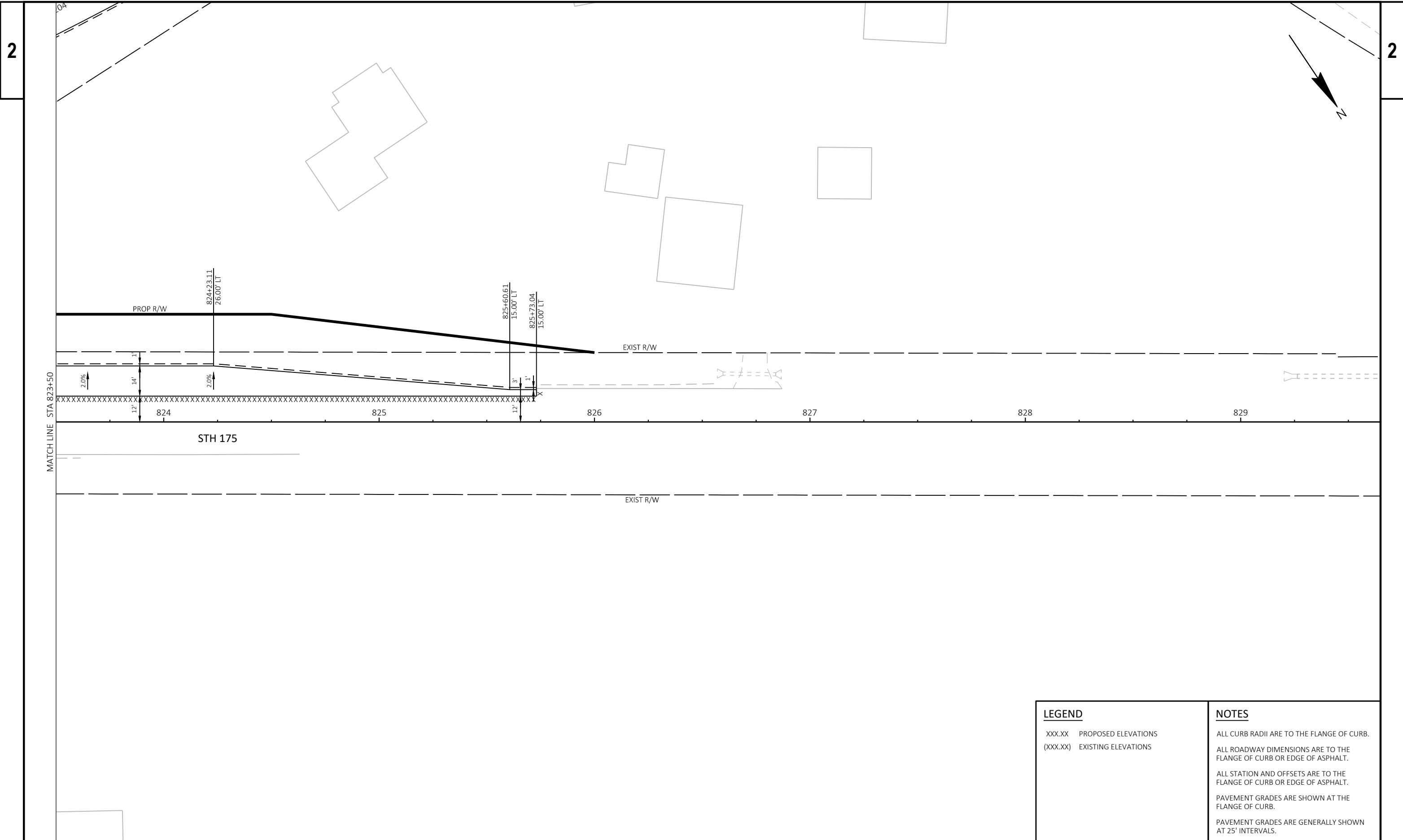
NOTES

ALL CURB RADII ARE TO THE FLANGE OF CURB.
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ALL STATION AND OFFSETS ARE TO THE FLANGE OF CURB OR EDGE OF ASPHALT.
PAVEMENT GRADES ARE SHOWN AT THE FLANGE OF CURB.
PAVEMENT GRADES ARE GENERALLY SHOWN AT 25' INTERVALS.

2



| | | | | | |
|----------------------|------------|--------------------|------------------------|-------|---|
| PROJECT NO: HWY24-02 | HWY: CTH S | COUNTY: WASHINGTON | PLAN DETAILS - STH 175 | SHEET | E |
|----------------------|------------|--------------------|------------------------|-------|---|



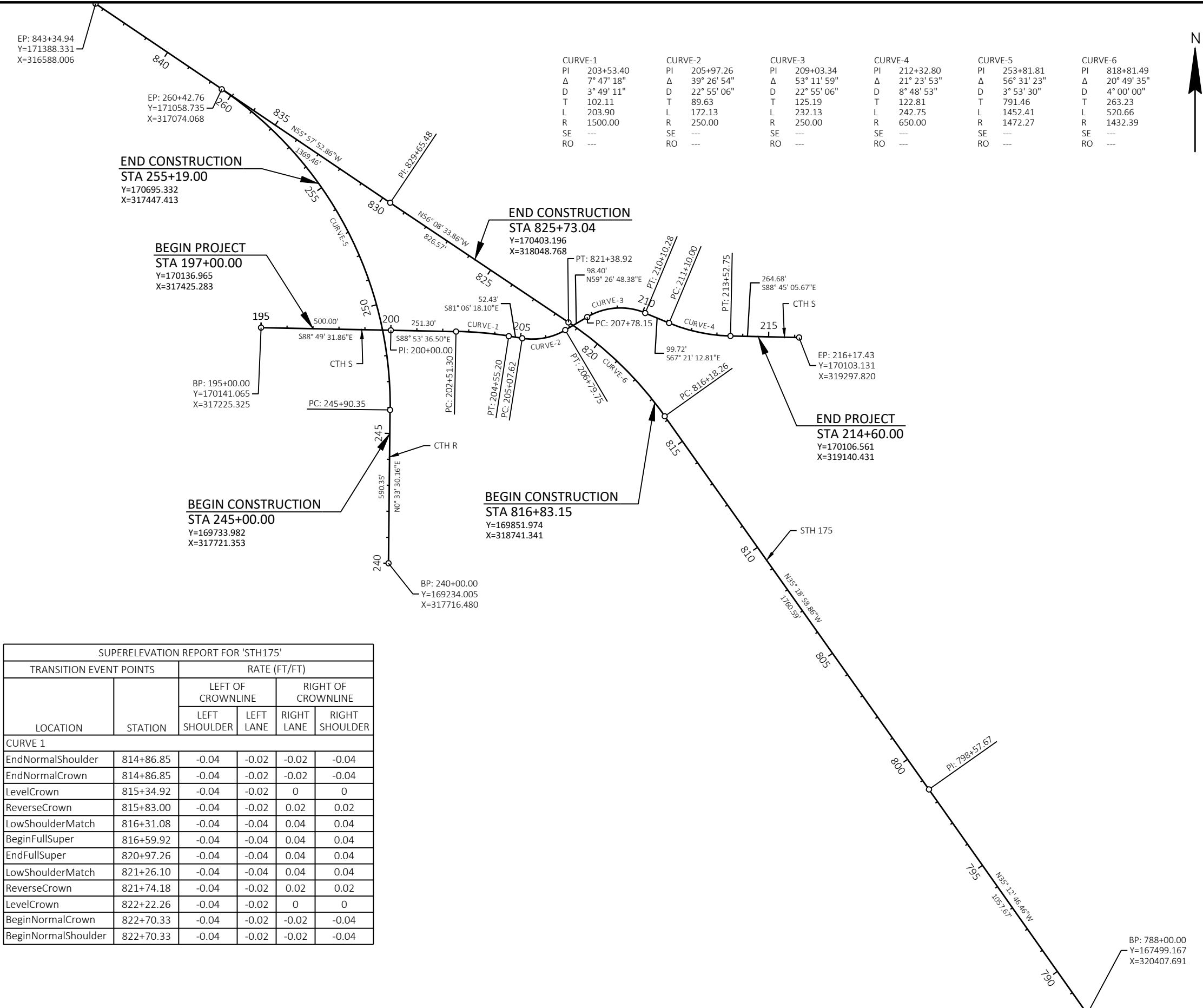
| LEGEND | NOTES |
|------------------------------|---|
| xxx.xx PROPOSED ELEVATIONS | ALL CURB RADII ARE TO THE FLANGE OF CURB. |
| (xxx.xx) EXISTING ELEVATIONS | ALL ROADWAY DIMENSIONS ARE TO THE FLANGE OF CURB OR EDGE OF ASPHALT. |
| | ALL STATION AND OFFSETS ARE TO THE FLANGE OF CURB OR EDGE OF ASPHALT. |
| | PAVEMENT GRADES ARE SHOWN AT THE FLANGE OF CURB. |
| | PAVEMENT GRADES ARE GENERALLY SHOWN AT 25' INTERVALS. |

| SUPERELEVATION REPORT FOR 'CTHS' | | | | | |
|----------------------------------|-----------|------------------|-----------|-------------------|----------------|
| TRANSITION EVENT POINTS | | RATE (FT/FT) | | | |
| LOCATION | STATION | LEFT OF CROWNLIN | | RIGHT OF CROWNLIN | |
| | | LEFT SHOULDER | LEFT LANE | RIGHT LANE | RIGHT SHOULDER |
| CURVE 1 | | | | | |
| EndNormalShoulder | 201+68.47 | -0.04 | -0.02 | -0.02 | -0.04 |
| EndNormalCrown | 201+68.47 | -0.04 | -0.02 | -0.02 | -0.04 |
| LevelCrown | 202+07.29 | 0 | 0 | -0.02 | -0.04 |
| ReverseCrown | 202+46.12 | 0.02 | 0.02 | -0.02 | -0.04 |
| BeginFullSuper | 202+73.29 | 0.034 | 0.034 | -0.034 | -0.04 |
| EndFullSuper | 204+16.00 | 0.034 | 0.034 | -0.034 | -0.04 |
| ReverseCrown | 204+60.38 | 0.011 | 0.011 | -0.011 | -0.015 |
| LevelCrown | 204+99.20 | -0.009 | -0.009 | 0.009 | 0.007 |
| BeginNormalCrown | 205+38.02 | -0.03 | -0.03 | 0.03 | 0.029 |
| BeginNormalShoulder | 205+38.02 | -0.03 | -0.03 | 0.03 | 0.029 |
| CURVE 2 | | | | | |
| BeginFullSuper | 205+58.00 | -0.04 | -0.04 | 0.04 | 0.04 |
| EndFullSuper | 206+00.00 | -0.04 | -0.04 | 0.04 | 0.04 |
| ReverseCrown | 206+38.50 | -0.02 | -0.02 | 0.02 | 0.02 |
| CURVE 3 | | | | | |
| Manual | 207+40.00 | -0.02 | -0.02 | 0.02 | 0.02 |
| ReverseCrown | 208+21.00 | 0.02 | 0.02 | -0.02 | -0.02 |
| BeginFullSuper | 208+59.50 | 0.04 | 0.04 | -0.04 | -0.04 |
| EndFullSuper | 209+83.00 | 0.04 | 0.04 | -0.04 | -0.04 |
| CURVE 4 | | | | | |
| EndNormalShoulder | 210+05.17 | 0.029 | 0.029 | -0.029 | -0.029 |
| EndNormalCrown | 210+05.17 | 0.029 | 0.029 | -0.029 | -0.029 |
| LevelCrown | 210+43.99 | 0.008 | 0.008 | -0.008 | -0.008 |
| ReverseCrown | 210+82.82 | -0.012 | -0.012 | 0.012 | 0.012 |
| LowShoulderMatch | 211+21.64 | -0.032 | -0.032 | 0.032 | 0.032 |
| BeginFullSuper | 211+59.00 | -0.051 | -0.051 | 0.051 | 0.051 |
| EndFullSuper | 213+19.75 | -0.051 | -0.051 | 0.051 | 0.051 |
| LowShoulderMatch | 213+41.10 | -0.04 | -0.04 | 0.04 | 0.04 |
| ReverseCrown | 213+79.93 | -0.04 | -0.02 | 0.02 | 0.02 |
| LevelCrown | 214+18.75 | -0.04 | -0.02 | 0 | 0 |
| BeginNormalCrown | 214+57.58 | -0.04 | -0.02 | -0.02 | -0.04 |
| BeginNormalShoulder | 214+57.58 | -0.04 | -0.02 | -0.02 | -0.04 |

| SUPERELEVATION REPORT FOR 'CTHR' | | | | | |
|----------------------------------|-----------|------------------|-----------|-------------------|----------------|
| TRANSITION EVENT POINTS | | RATE (FT/FT) | | | |
| LOCATION | STATION | LEFT OF CROWNLIN | | RIGHT OF CROWNLIN | |
| | | LEFT SHOULDER | LEFT LANE | RIGHT LANE | RIGHT SHOULDER |
| CURVE 1 | | | | | |
| EndNormalShoulder | 244+43.94 | -0.04 | -0.02 | -0.02 | -0.04 |
| EndNormalCrown | 244+43.94 | -0.04 | -0.02 | -0.02 | -0.04 |
| LevelCrown | 244+95.01 | -0.04 | -0.02 | 0 | 0 |
| ReverseCrown | 245+46.08 | -0.04 | -0.02 | 0.02 | 0.02 |
| LowShoulderMatch | 245+97.15 | -0.04 | -0.04 | 0.04 | 0.04 |
| BeginFullSuper | 246+38.01 | -0.056 | -0.056 | 0.056 | 0.04 |
| Manual | 250+00.00 | -0.056 | -0.056 | 0.056 | 0.04 |
| Manual | 250+39.07 | -0.056 | -0.056 | 0.056 | 0.056 |
| Manual | 252+61.00 | -0.056 | -0.056 | 0.056 | 0.056 |
| Manual | 255+19.00 | -0.056 | -0.056 | -0.045 | -0.045 |
| EndFullSuper | 259+95.10 | -0.056 | -0.056 | -0.045 | -0.045 |
| LowShoulderMatch | 260+35.96 | -0.04 | -0.04 | -0.045 | -0.045 |
| ReverseCrown | 260+87.03 | -0.04 | -0.02 | -0.045 | -0.045 |
| LevelCrown | 261+38.10 | -0.04 | -0.02 | -0.045 | -0.045 |
| BeginNormalCrown | 261+89.17 | -0.04 | -0.02 | -0.045 | -0.045 |
| BeginNormalShoulder | 261+89.17 | -0.04 | -0.02 | -0.045 | -0.045 |

| SUPERELEVATION REPORT FOR 'STH175' | | | | | |
|------------------------------------|-----------|------------------|-----------|-------------------|----------------|
| TRANSITION EVENT POINTS | | RATE (FT/FT) | | | |
| LOCATION | STATION | LEFT OF CROWNLIN | | RIGHT OF CROWNLIN | |
| | | LEFT SHOULDER | LEFT LANE | RIGHT LANE | RIGHT SHOULDER |
| CURVE 1 | | | | | |
| EndNormalShoulder | 814+86.85 | -0.04 | -0.02 | -0.02 | -0.04 |
| EndNormalCrown | 814+86.85 | -0.04 | -0.02 | -0.02 | -0.04 |
| LevelCrown | 815+34.92 | -0.04 | -0.02 | 0 | 0 |
| ReverseCrown | 815+83.00 | -0.04 | -0.02 | 0.02 | 0.02 |
| LowShoulderMatch | 816+31.08 | -0.04 | -0.04 | 0.04 | 0.04 |
| BeginFullSuper | 816+59.92 | -0.04 | -0.04 | 0.04 | 0.04 |
| EndFullSuper | 820+97.26 | -0.04 | -0.04 | 0.04 | 0.04 |
| LowShoulderMatch | 821+26.10 | -0.04 | -0.04 | 0.04 | 0.04 |
| ReverseCrown | 821+74.18 | -0.04 | -0.02 | 0.02 | 0.02 |
| LevelCrown | 822+22.26 | -0.04 | -0.02 | 0 | 0 |
| BeginNormalCrown | 822+70.33 | -0.04 | -0.02 | -0.02 | -0.04 |
| BeginNormalShoulder | 822+70.33 | -0.04 | -0.02 | -0.02 | -0.04 |

| CURVE-1 | CURVE-2 | CURVE-3 | CURVE-4 | CURVE-5 | CURVE-6 |
|--------------|---------------|---------------|---------------|---------------|---------------|
| PI 203+53.40 | PI 205+97.26 | PI 209+03.34 | PI 212+32.80 | PI 253+81.81 | PI 818+81.49 |
| Δ 7° 47' 18" | Δ 39° 26' 54" | Δ 53° 11' 59" | Δ 21° 23' 53" | Δ 56° 31' 23" | Δ 20° 49' 35" |
| D 3° 49' 11" | D 22° 55' 06" | D 22° 55' 06" | D 8° 48' 53" | D 3° 53' 30" | D 4° 00' 00" |
| T 102.11 | T 89.63 | T 125.19 | T 122.81 | T 791.46 | T 263.23 |
| L 203.90 | L 172.13 | L 232.13 | L 242.75 | L 1452.41 | L 520.66 |
| R 1500.00 | R 250.00 | R 250.00 | R 650.00 | R 1472.27 | R 1432.39 |
| SE --- | SE --- | SE --- | SE --- | SE --- | SE --- |
| RO --- | RO --- | RO --- | RO --- | RO --- | RO --- |



CONVENTIONAL SYMBOLS AND ABBREVIATIONS

| | |
|----------------------------|-------|
| SECTION LINE | ---- |
| QUARTER LINE | ---- |
| SIXTEENTH LINE | ---- |
| NEW REFERENCE LINE | ---- |
| NEW R/W LINE | ---- |
| EXISTING R/W LINE | ---- |
| PROPERTY LINE | ---- |
| CORPORATE LIMITS | //// |
| LOT AND TIE LINES | ---- |
| SLOPE INTERCEPT | ---- |
| UNDERGROUND FACILITY | ---- |
| FENCE | -x-x- |
| LIMITED EASEMENT | ---- |
| BUILDING | □ |
| IRON PIN FOUND | oIP |
| R/W MONUMENT | ● |
| BUSHES | ☁ |
| TREES (DECIDUOUS) | ⊙ |
| TREES (CONIFEROUS) | ⊙ |
| POWER POLE (COMPENSABLE) | + |
| SIGN | + |
| POWER POLE | o |
| UTILITY PEDESTAL | o |
| VALVE | o |
| POINT NUMBER | ○ |
| RECORDED AS | ○ |
| SAME OWNERSHIP | ○ |
| NO ACCESS (BY ACQUISITION) | |
| NO ACCESS (PREVIOUS) | |
| TEMPORARY LIMITED EASEMENT | ---- |
| SECTION CORNER | + |

| | |
|--------|----------------------------|
| AC. | ACRES |
| A.P. | ACCESS POINT |
| BLDG. | BUILDING |
| C/L | CENTERLINE |
| CONC. | CONCRETE |
| CSM | CERTIFIED SURVEY MAP |
| D | DEGREE OF CURVE |
| DOC. # | DOCUMENT NUMBER |
| E | EAST |
| E | ELECTRIC CABLE |
| ETAL | AND OTHERS |
| FM | FORCE MAIN |
| FO | FIBER OPTIC CABLE |
| FT. | FEET |
| GRGE | GARAGE |
| G | GAS MAIN |
| HSE | HOUSE |
| H.E. | HIGHWAY EASEMENT |
| IP | IRON PIPE |
| L | LENGTH OF CURVE |
| LC | LAND CONTRACT |
| LC | LONG CHORD |
| LCB | LONG CHORD BEARING |
| LT. | LEFT |
| MI. | MILE |
| MON. | MONUMENT |
| N | NORTH |
| OL | OUTLOT |
| P.C. | POINT OF CURVE |
| PERM. | PERMANENT |
| P.I. | POINT OF INTERSECTION |
| P.L. | PROPERTY LINE |
| P.L.E. | PERMANENT LIMITED EASEMENT |
| P.T. | POINT OF TANGENT |
| R | RADIUS |
| R | RANGE |
| R/L | REFERENCE LINE |
| REM. | REMAINING |
| RT. | RIGHT |
| R/W | RIGHT OF WAY |
| RWGP | RIGHT OF WAY GUARD POST |
| S | SOUTH |
| SAN | SANITARY SEWER |
| SO.FT. | SQUARE FEET |
| SS | STORM SEWER |
| STA. | STATION |
| T | TELEPHONE |
| T | TANGENT |
| T | TOWNSHIP |
| TEMP. | TEMPORARY |
| TLE | TEMPORARY LIMITED EASEMENT |
| V/P | VOLUME/PAGE OF RECORDS |
| VAR. | VARIES |
| W | WATER MAIN |
| W | WEST |
| X | EAST COORDINATE |
| Y | NORTH COORDINATE |

NOTES

COORDINATES AND BEARINGS ON THIS PLAT ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), WASHINGTON COUNTY ZONE, NAD83 (2011) ADJUSTMENT. THE COORDINATES SHOWN ARE GRID COORDINATES AND ARE TO BE USED AS GRID OR GROUND VALUES ON THIS PLAT.

RIGHT OF WAY MONUMENTS ARE TYPE 2 AND ARE PLACED PRIOR TO OR AT THE TIME OF LAND TITLE TRANSFER. TYPE 2 MONUMENTS PLACED ARE 1.32" O.D. x 24" IRON PIPES WEIGHING 1.68 LBS/FT., UNLESS OTHERWISE NOTED.

RIGHT OF WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER SURVEYS OF PUBLIC RECORD.

A PERMANENT LIMITED EASEMENT (PLE) IS A RIGHT FOR CONSTRUCTION AND MAINTENANCE PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE, BUT WITHOUT PREJUDICE TO THE OWNER'S RIGHTS TO MAKE OR CONSTRUCT IMPROVEMENTS ON SAID LANDS OR TO FLATTEN THE SLOPES, PROVIDING SAID ACTIVITIES WILL NOT IMPAIR OR OTHERWISE ADVERSELY AFFECT THE HIGHWAY FACILITIES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

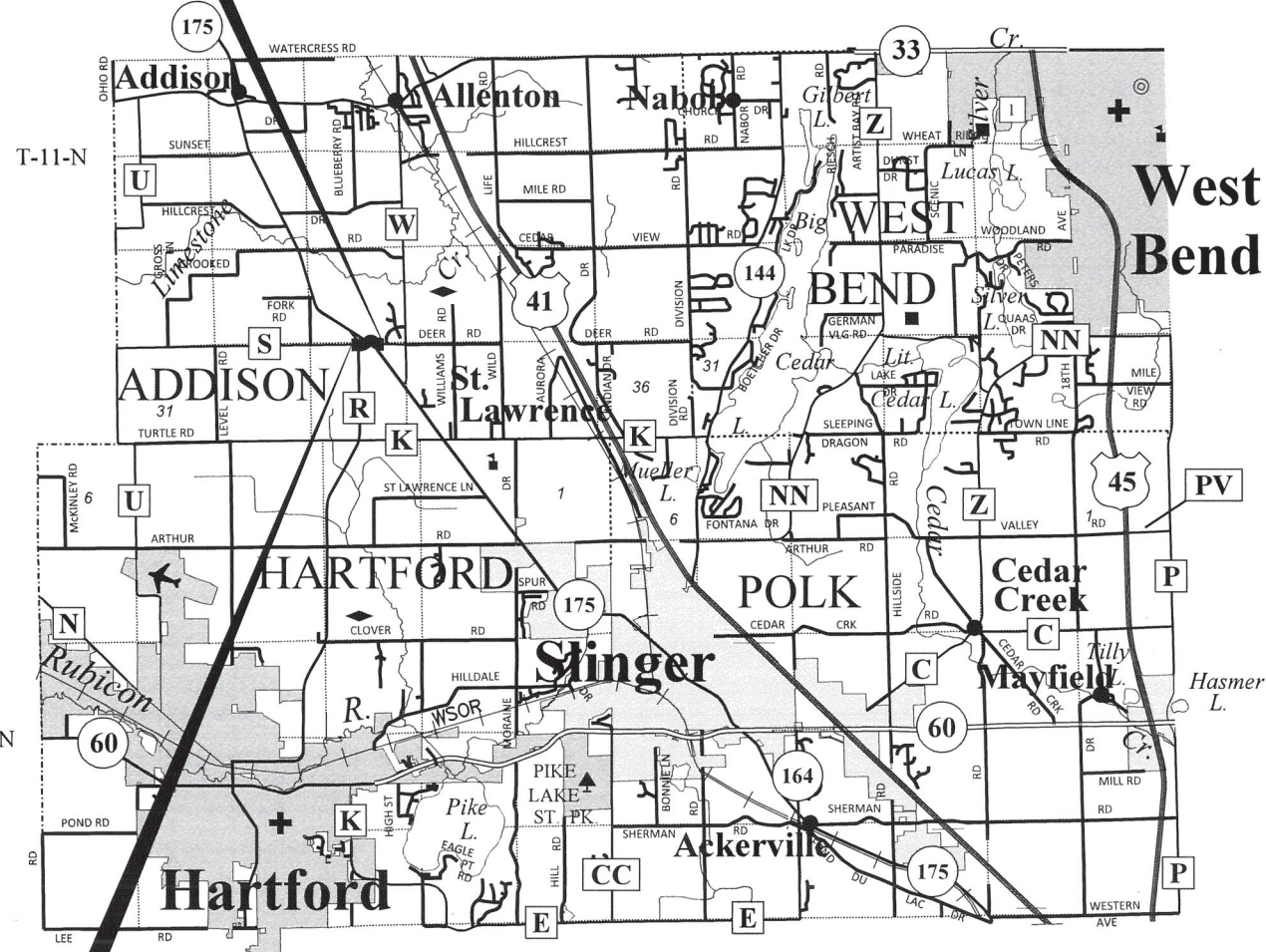
PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES.

THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

END RELOCATION ORDER

STA. 213+62.63

22.13' SOUTH AND 1317.86' EAST OF THE NORTH QUARTER CORNER OF SECTION 33, T11N, R18E
Y 170108.682
X 319043.084

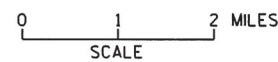


BEGIN RELOCATION ORDER

STA. 198+17.62

BEING 3.74' NORTH AND 182.34' WEST OF THE SOUTH QUARTER CORNER OF SECTION 28, T11N, R18E
Y 170134.554
X 317542.879

LAYOUT



TOTAL NET LENGTH OF CENTERLINE = 0.293 MILES

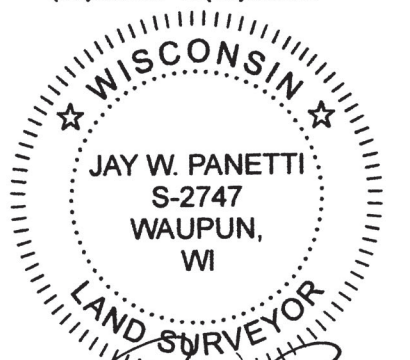
| | | | |
|-----------------------------------|-----------|-------------------|--------------|
| R/W PROJECT NUMBER | HWY 24-02 | SHEET NUMBER | TOTAL SHEETS |
| FEDERAL PROJECT NUMBER | | 4.01 | 6 |
| PLAT OF RIGHT OF WAY REQUIRED FOR | | | |
| STH 175, CTH S, CTH R | | | |
| INTERSECTION IMPROVEMENTS | | | |
| TOWN OF ADDISON | | WASHINGTON COUNTY | |
| CONSTRUCTION PROJECT NUMBER | | | |
| HWY 24-02 | | | |

ACCEPTED FOR WASHINGTON COUNTY

DATE SCOTT SCHMIDT
CHIEF PUBLIC WORKS OFFICER

ORIGINAL PLANS PREPARED BY

G GREMMER & ASSOCIATES, INC.
CONSULTING ENGINEERS
Stevens Point • Fond du Lac
85 South Pioneer Road, Suite 300 • Fond du Lac, WI 54605
(920) 824-5720 • fax (920) 824-5725



5/11/23
DATE JAY W. PANETTI, PLS

| REVISION | DATE |
|----------|------|
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| | |
| | |

SCHEDULE OF LANDS & INTERESTS REQUIRED

AREAS SHOWN ON THE TOTAL ACRES COLUMN MAY BE APPROXIMATE AND ARE DERIVED FROM TAX ROLLS OR OTHER AVAILABLE SOURCES AND MAY NOT INCLUDE LANDS OF THE OWNER WHICH ARE NOT CONTIGUOUS TO THE AREA TO BE ACQUIRED. OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.

| PARCEL NUMBER | SHEET NUMBER | OWNER(S) | INTEREST REQUIRED | TOTAL ACRES (EXISTING) | R/W ACRES REQUIRED | | | TOTAL ACRES REMAINING | P.L.E. ACRES PERM. | T.L.E. ACRES TEMP. | TAX KEY NUMBER |
|---------------|--------------|---|-------------------|------------------------|--------------------|----------|-------|-----------------------|--------------------|--------------------|----------------|
| | | | | | NEW | EXISTING | TOTAL | | | | |
| 1 | 4.04 | GEGHF, LLC | FEE | 55.50 | 0.10 | 0.36 | 0.46 | 55.04 | 0 | 0 | T1_0674 |
| | | | FEE | 24.11 | 0.04 | 0.05 | 0.09 | 24.02 | 0 | 0 | T1_0675 |
| | | | TOTALS: | 79.61 | 0.14 | 0.41 | 0.55 | 79.06 | 0 | 0 | |
| 2 | 4.05 | MARY L. SCHLEGEL | FEE | 1.08 | 0.03 | 0 | 0.03 | 1.05 | 0 | 0 | T1_067800K |
| 3 | 4.05 | SHELLY A. CHRISTMAN | PLE, TLE | 0.97 | 0 | 0 | 0 | 0.97 | 0.03 | 0.02 | T1_067800H |
| 4 | 4.05 | CHRISTOPHER W. GERHARDT | FEE, TLE | 1.98 | 0.18 | 0 | 0.18 | 1.80 | 0 | 0.07 | T1_076200A |
| 6 | 4.05 | DONNA L. SCOTT | FEE | 0.95 | 0.13 | 0 | 0.13 | 0.82 | 0 | 0 | T1_067800J |
| 7 | 4.05 | GEGF, PROPERTIES, LLC | FEE, TLE | 33.81 | 0.03 | 0.08 | 0.11 | 33.70 | 0 | 0.02 | T1_076200Z |
| 8 | 4.05 | DALE A. FIES AND KATHLEEN S. FIES | FEE, TLE | 0.99 | 0.15 | 0 | 0.15 | 0.84 | 0 | 0.02 | T1_067800E |
| 9 | 4.05 | ROMAN H. BECKER AND CHRISTINE M. BECKER | FEE | 28.81 | 0.75 | 0.40 | 1.15 | 27.66 | 0 | 0 | T1_067800W |
| 11 | 4.05 | STANLEY G. OJSTRSEK | FEE | 2.68 | 0.12 | 0 | 0.12 | 2.56 | 0 | 0 | T1_0761 |
| 201 | 4.05 | FRONTIER COMMUNICATIONS | RELEASE OF RIGHTS | | | | | | | | |
| 202 | 4.05 | WE ENERGIES - ELECTRIC | RELEASE OF RIGHTS | | | | | | | | |

4

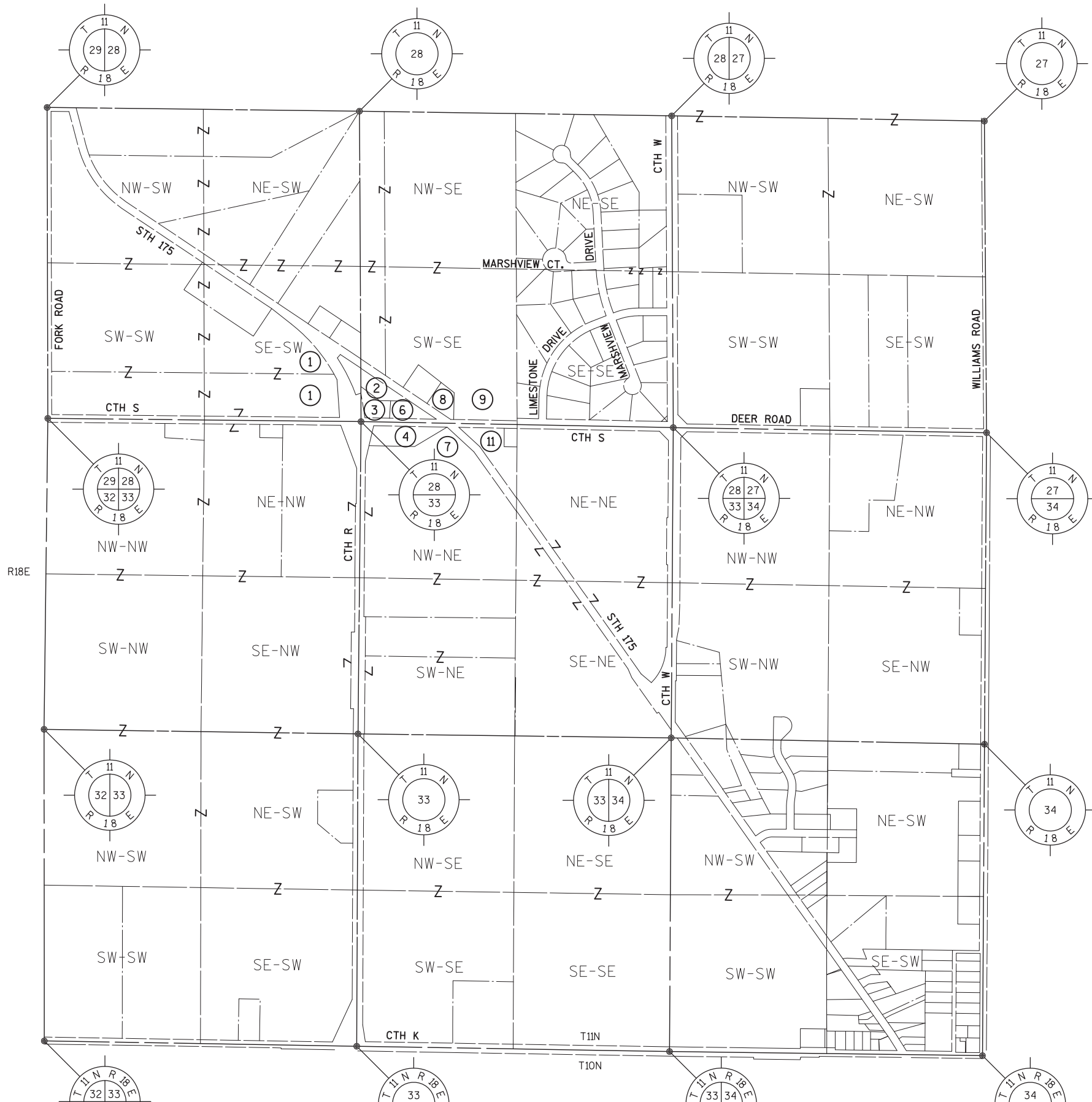
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REVISED: 5/11/2023

| | | | | |
|---------------|-----------------|----------------------------|--|------------------|
| REVISION DATE | DATE: 6/15/2023 | HWY: STH 175, CTH S, CTH R | R/W PROJECT NUMBER: HWY 24-02 | PLAT SHEET: 4.02 |
| | | COUNTY: WASHINGTON | CONSTRUCTION PROJECT NUMBER: HWY 24-02 | PS&E SHEET: E |

4

4



TOWN OF ADDISON
WASHINGTON COUNTY, WISCONSIN

SHEET 3 OF 6 SHEETS

REVISED: 5/11/2023

| | | | | | |
|---------------|-----------------|-------------------------------|----------------------------|--|------------------|
| REVISION DATE | DATE: 6/15/2023 | SCALE, FEET 0 250 500 1000 | HWY: STH 175, CTH S, CTH R | R/W PROJECT NUMBER: HWY 24-02 | PLAT SHEET: 4.03 |
| ----- | | | COUNTY: WASHINGTON | CONSTRUCTION PROJECT NUMBER: HWY 24-02 | PS&E SHEET: E |

COORDINATE TABLE

| POINT | NORTH | EAST |
|-------|-------------|-------------|
| 400 | 170132.6779 | 317542.8595 |
| 401 | 170165.6761 | 317543.1964 |
| 402 | 170421.0461 | 317525.6525 |
| 403 | 170523.4572 | 317485.1767 |
| 404 | 170642.1854 | 317411.4450 |
| 405 | 170804.8252 | 317299.7767 |
| 406 | 170835.3013 | 317332.8897 |
| 408 | 170509.5200 | 317563.3102 |
| 409 | 170487.9765 | 317521.0544 |

PARCEL 1 - FEE

| FROM POINT | TO POINT | BEARING | DISTANCE |
|------------|----------|-------------|----------|
| 32 | 400 | N89°24'54"W | 182.37' |
| 400 | 401 | N00°35'06"E | 33.00' |
| 401 | 402 | N03°55'48"W | 255.97' |
| 402 | 403 | N21°33'55"W | 110.12' |
| 403 | 404 | N31°50'27"W | 139.76' |
| 404 | 405 | N34°28'24"W | 197.29' |
| 405 | 406 | N47°22'28"E | 45.00' |
| *406 | *408 | S35°16'16"E | 399.03' |
| 408 | 409 | S62°59'09"W | 47.43' |
| 409 | 402 | S03°55'48"E | 67.09' |

*SEE CURVE TABLE

CURVE TABLE

| FROM POINT | TO POINT | ARC RADIUS | ARC LENGTH | CHORD BEARING | CHORD LENGTH |
|------------|----------|------------|------------|---------------|--------------|
| 406 | 408 | 1432.39' | 400.33' | S35°16'16"E | 399.03' |

R/L CURVE DATA

| | |
|-----|-------------|
| PI | 253+81.81 |
| Y | 170615.751 |
| X | 317729.946 |
| Δ | 056°31'23" |
| D | 3°53'30" |
| T | 791.46' |
| L | 1452.41' |
| R | 1472.27' |
| PC | 245+90.35 |
| Y | 169824.327 |
| X | 317722.233 |
| PT | 260+42.76 |
| Y | 171058.735 |
| X | 317074.068 |
| LC | 1394.23' |
| LCB | N27°42'11"W |

BASIS FOR EXISTING R/W

| ROUTE | BASIS |
|---------|---|
| STH 175 | STATE AID PROJ. NO. 511-A, CSM #2134, CSM #2804 |
| CTH S | R/W PROJECT NUMBER 1330-00-20, CSM #1168, CSM #4105, CSM #7234 |
| CTH R | STATE AID PROJ. NO. 511-A, R/W PROJECT NUMBER 1330-00-20, CSM #4751 |

NOTE - PROPERTY LINE STATIONS ARE COMPUTED FROM INFORMATION OF RECORD AND ARE APPROXIMATE ONLY.

SE-SW

SECTION 28, T11N, R18E

GN

TOWN OF ADDISON
WASHINGTON COUNTY, WISCONSIN

T1.0674
GEGHF, LLC
DOC. #1056241

T1.0675
GEGHF, LLC
DOC. #1056241

BEGIN RELOCATION ORDER

198+17.62
Y 170134.554
X 317542.879
3.74' NORTH AND 182.34' WEST
OF THE SOUTH QUARTER CORNER
OF SECTION 28, T11N, R18E

BRASS CAP
Y 172766.876
X 317711.805

BRASS CAP
Y 170130.816
X 317725.220

SEE SHEET 4.05

SEE SHEET 4.05

SHEET 4 OF 6 SHEETS

REVISED: 5/11/2023

| REVISION DATE | DESCRIPTION |
|---------------|-------------|
| | |
| | |
| | |

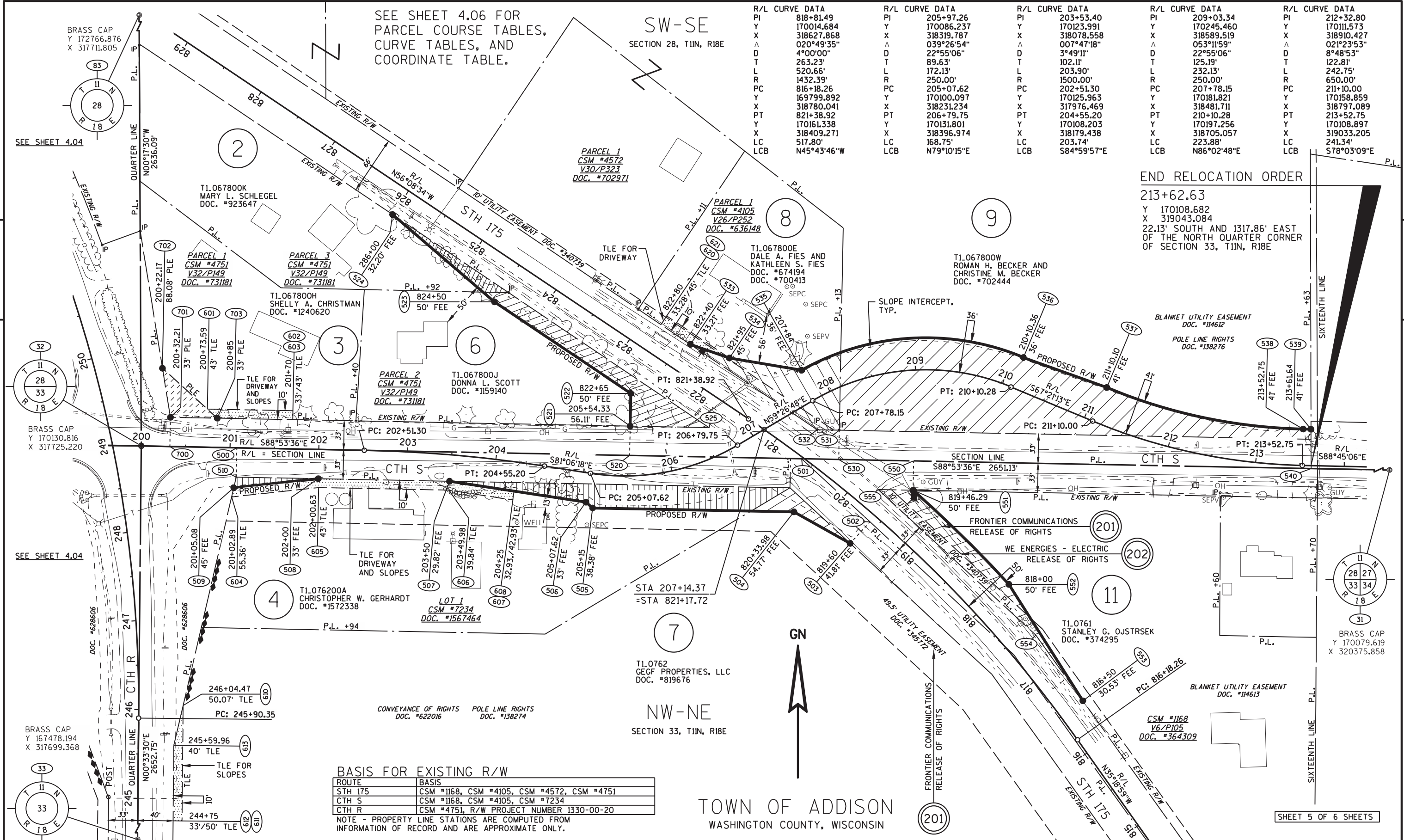
DATE: 6/15/2023
GRID FACTOR N/A



HWY: STH 175, CTH S, CTH R
COUNTY: WASHINGTON

R/W PROJECT NUMBER: HWY 24-02
CONSTRUCTION PROJECT NUMBER: HWY 24-02

PLAT SHEET: 4.04
PS&E SHEET: E



SEE SHEET 4.06 FOR
PARCEL COURSE TABLES,
CURVE TABLES, AND
COORDINATE TABLE.

| R/L CURVE DATA | R/L CURVE DATA | R/L CURVE DATA | R/L CURVE DATA | R/L CURVE DATA |
|-----------------|-----------------|-----------------|-----------------|-----------------|
| PI 818+81.49 | PI 205+97.26 | PI 203+53.40 | PI 209+03.34 | PI 212+32.80 |
| Y 170014.684 | Y 170086.237 | Y 170123.991 | Y 170245.460 | Y 170111.573 |
| X 318627.868 | X 318319.787 | X 318078.558 | X 318589.519 | X 318910.427 |
| Δ 020°49'35" | Δ 039°26'54" | Δ 007°47'18" | Δ 053°11'59" | Δ 021°23'53" |
| D 4°00'00" | D 22°55'06" | D 3°49'11" | D 22°55'06" | D 8°48'53" |
| T 263.23' | T 89.63' | T 102.11' | T 125.19' | T 122.81' |
| L 520.66' | L 172.13' | L 203.90' | L 232.13' | L 242.75' |
| R 1432.39' | R 250.00' | R 1500.00' | R 250.00' | R 650.00' |
| PC 816+18.26 | PC 205+07.62 | PC 202+51.30 | PC 207+78.15 | PC 211+10.00 |
| Y 169799.892 | Y 170100.097 | Y 170125.963 | Y 170181.821 | Y 170158.859 |
| X 318780.041 | X 318231.234 | X 317976.469 | X 318481.711 | X 318797.089 |
| PT 821+38.92 | PT 206+79.75 | PT 204+55.20 | PT 210+10.28 | PT 213+52.75 |
| Y 170161.338 | Y 170131.801 | Y 170108.203 | Y 170197.256 | Y 170108.897 |
| X 318409.271 | X 318396.974 | X 318179.438 | X 318705.057 | X 319033.205 |
| LC 517.80' | LC 168.75' | LC 203.74' | LC 223.88' | LC 241.34' |
| LCB N45°43'46"W | LCB N79°10'15"E | LCB S84°59'57"E | LCB N86°02'48"E | LCB S78°03'09"E |

END RELOCATION ORDER
213+62.63
Y 170108.682
X 319043.084
22.13' SOUTH AND 1317.86' EAST
OF THE NORTH QUARTER CORNER
OF SECTION 33, T11N, R18E

BASIS FOR EXISTING R/W

| ROUTE | BASIS |
|---------|--|
| STH 175 | CSM #1168, CSM #4105, CSM #4572, CSM #4751 |
| CTH S | CSM #1168, CSM #4105, CSM #7234 |
| CTH R | CSM #4751, R/W PROJECT NUMBER 1330-00-20 |

NOTE - PROPERTY LINE STATIONS ARE COMPUTED FROM INFORMATION OF RECORD AND ARE APPROXIMATE ONLY.

TOWN OF ADDISON
WASHINGTON COUNTY, WISCONSIN

| | | | | | |
|---------------|-----------------|-------------|----------------------------|--|------------------|
| REVISION DATE | DATE: 6/15/2023 | SCALE, FEET | HWY: STH 175, CTH S, CTH R | R/W PROJECT NUMBER: HWY 24-02 | PLAT SHEET: 4.05 |
| | GRID FACTOR N/A | 0 50 100 | COUNTY: WASHINGTON | CONSTRUCTION PROJECT NUMBER: HWY 24-02 | PS&E SHEET: E |

PARCEL COURSE TABLES,
CURVE TABLES, AND
COORDINATE TABLE SHOWN
ARE FOR SHEET 4.05

PARCELS 2 & 6 - FEE

| FROM POINT | TO POINT | BEARING | DISTANCE |
|------------|----------|-------------|----------|
| 32 | 520 | S88°53'36"E | 550.25' |
| 520 | 521 | N01°06'24"E | 33.00' |
| 521 | 522 | N01°06'23"E | 36.88' |
| 522 | 523 | N56°08'34"W | 185.00' |
| 523 | 524 | N49°22'29"W | 151.05' |
| 524 | 525 | S56°02'57"E | 429.76' |
| 525 | 521 | N88°53'36"W | 88.95' |

PARCELS 4 & 7 - FEE

| FROM POINT | TO POINT | BEARING | DISTANCE |
|------------|----------|-------------|----------|
| 32 | 500 | S88°53'36"E | 107.63' |
| 500 | 501 | S88°53'36"E | 636.49' |
| 501 | 502 | S47°16'51"E | 104.80' |
| 502 | 503 | S42°43'09"W | 33.00' |
| 503 | 504 | N60°44'38"W | 72.64' |
| 504 | 505 | N88°53'36"W | 226.76' |
| 505 | 506 | N49°23'16"W | 10.00' |
| 506 | 507 | N81°15'52"W | 155.45' |
| 507 | 508 | N88°53'36"W | 147.97' |
| 508 | 509 | S83°54'04"W | 95.67' |
| 509 | 510 | N13°05'18"E | 12.27' |
| 510 | 500 | N01°06'24"E | 33.00' |

PARCELS 8 & 9 - FEE

| FROM POINT | TO POINT | BEARING | DISTANCE |
|------------|----------|-------------|----------|
| 32 | 530 | S88°53'36"E | 785.88' |
| 530 | 531 | N01°06'24"E | 33.00' |
| 531 | 532 | N88°53'36"W | 24.99' |
| 532 | 533 | N56°02'57"W | 172.11' |
| 533 | 534 | S70°49'09"E | 46.52' |
| 534 | 535 | S80°23'14"E | 82.61' |
| *535 | *536 | N86°43'38"E | 250.20' |
| 536 | 537 | S70°13'26"E | 99.85' |
| *537 | *538 | S78°03'26"E | 226.02' |
| 538 | 539 | S88°45'06"E | 8.89' |
| 539 | 540 | S00°07'52"E | 44.33' |
| 540 | 530 | N88°53'36"W | 532.24' |

*SEE CURVE TABLE

CURVE TABLE - PARCELS 8 & 9

| FROM POINT | TO POINT | RADIUS | ARC LENGTH | CHORD BEARING | CHORD LENGTH |
|------------|----------|---------|------------|---------------|--------------|
| 535 | 536 | 286.00' | 258.96' | N86°43'38"E | 250.20' |
| 537 | 538 | 609.00' | 227.34' | S78°03'26"E | 226.02' |

COORDINATE TABLE

| POINT | NORTH | EAST |
|-------|-------------|-------------|
| 500 | 170128.7375 | 317832.8299 |
| 501 | 170116.4460 | 318469.2018 |
| 502 | 170045.3489 | 318546.1977 |
| 503 | 170021.1041 | 318523.8104 |
| 504 | 170056.6041 | 318460.4363 |
| 505 | 170060.9831 | 318233.7235 |
| 506 | 170067.4935 | 318226.1310 |
| 507 | 170091.1024 | 318072.4859 |
| 508 | 170093.9599 | 317924.5454 |
| 509 | 170083.7951 | 317829.4147 |
| 510 | 170095.7437 | 317832.1927 |
| 520 | 170120.1900 | 318275.3632 |
| 521 | 170153.1838 | 318276.0005 |
| 522 | 170190.0601 | 318276.7127 |
| 523 | 170293.1284 | 318123.0835 |
| 524 | 170391.4800 | 318008.4369 |
| 525 | 170151.4661 | 318364.9334 |
| 530 | 170115.6396 | 318510.9534 |
| 531 | 170148.6334 | 318511.5907 |
| 532 | 170149.1161 | 318486.6020 |
| 533 | 170245.2355 | 318343.8343 |
| 534 | 170229.9520 | 318387.7697 |
| 535 | 170216.1579 | 318469.2158 |
| 536 | 170230.4419 | 318119.0105 |
| 537 | 170196.6594 | 318812.9683 |
| 538 | 170149.8877 | 319034.0986 |
| 539 | 170149.6940 | 319042.9900 |
| 540 | 170105.3614 | 319043.0913 |
| 550 | 170114.0093 | 318595.3564 |
| 551 | 170081.0155 | 318594.7191 |
| 552 | 169974.9682 | 318702.6781 |
| 553 | 169843.7817 | 318785.9200 |
| 554 | 169917.7316 | 318733.0467 |
| 555 | 170081.7751 | 318555.3929 |
| 601 | 170172.3868 | 317799.6288 |
| 602 | 170170.5250 | 317896.0187 |
| 603 | 170160.5269 | 317895.8256 |
| 604 | 170073.4820 | 317827.0170 |
| 605 | 170083.9496 | 317924.9818 |
| 606 | 170081.1189 | 318071.6151 |
| 607 | 170070.0308 | 318143.7757 |
| 608 | 170079.9411 | 318145.1229 |
| 610 | 169838.4437 | 317772.3730 |
| 611 | 169708.4956 | 317771.1065 |
| 612 | 169708.5931 | 317761.1070 |
| 613 | 169793.5471 | 317761.9349 |
| 620 | 170267.5747 | 318310.6535 |
| 621 | 170277.3077 | 318317.1833 |
| 700 | 170130.1940 | 317757.4238 |
| 701 | 170163.1878 | 317758.0611 |
| 702 | 170218.4482 | 317749.0857 |
| 703 | 170162.1684 | 317810.8414 |

PARCEL 3 - PLE

| FROM POINT | TO POINT | BEARING | DISTANCE |
|------------|----------|-------------|----------|
| 32 | 700 | S88°53'36"E | 32.21' |
| 700 | 701 | N01°06'24"E | 33.00' |
| 701 | 702 | N09°13'32"W | 55.98' |
| 702 | 703 | S47°39'22"E | 83.55' |
| 703 | 701 | N88°53'36"W | 52.79' |

PARCEL 4 - TLE

| FROM POINT | TO POINT | BEARING | DISTANCE |
|------------|----------|-------------|----------|
| 32 | 500 | S88°53'36"E | 107.63' |
| 500 | 510 | S01°06'24"W | 33.00' |
| 510 | 509 | S13°05'18"W | 12.27' |
| 509 | 508 | N83°54'04"E | 95.67' |
| 508 | 507 | S88°53'36"E | 147.97' |
| 507 | 608 | S81°15'52"E | 73.49' |
| 608 | 607 | S07°44'30"W | 10.00' |
| 607 | 606 | N81°15'52"W | 73.01' |
| 606 | 605 | N88°53'39"W | 146.66' |
| 605 | 604 | S83°54'04"W | 98.52' |
| 604 | 509 | N13°05'17"E | 10.59' |

PARCEL 8 - TLE

| FROM POINT | TO POINT | BEARING | DISTANCE |
|------------|----------|-------------|----------|
| 32 | 530 | S88°53'36"E | 785.88' |
| 530 | 531 | N01°06'24"E | 33.00' |
| 531 | 532 | N88°53'36"W | 24.99' |
| 532 | 533 | N56°02'57"W | 172.11' |
| 533 | 620 | N56°02'57"W | 40.00' |
| 620 | 621 | N33°51'26"E | 11.72' |
| 621 | 534 | S56°08'34"E | 85.00' |
| 534 | 533 | N70°49'09"W | 46.52' |

PARCEL 3 - TLE

| FROM POINT | TO POINT | BEARING | DISTANCE |
|------------|----------|-------------|----------|
| 32 | 700 | S88°53'36"E | 32.21' |
| 700 | 701 | N01°06'24"E | 33.00' |
| 701 | 703 | S88°53'36"E | 52.79' |
| 703 | 601 | N47°39'22"W | 15.17' |
| 601 | 602 | S88°53'36"E | 96.41' |
| 602 | 603 | S01°06'24"W | 10.00' |
| 603 | 703 | N88°53'36"W | 85.00' |

PARCEL 7 - TLE

| FROM POINT | TO POINT | BEARING | DISTANCE |
|------------|----------|-------------|----------|
| 32 | 500 | S88°53'36"E | 107.63' |
| 500 | 510 | S01°06'24"W | 33.00' |
| 510 | 610 | S13°05'18"W | 264.16' |
| 610 | 611 | S00°33'30"W | 129.95' |
| 611 | 612 | N89°26'30"W | 10.00' |
| 612 | 613 | N00°33'30"E | 84.96' |
| 613 | 610 | N13°05'18"E | 46.09' |

PARCEL 11 - FEE

| FROM POINT | TO POINT | BEARING | DISTANCE |
|------------|----------|-------------|----------|
| 32 | 550 | S88°53'36"E | 870.30' |
| 550 | 551 | S01°06'24"W | 33.00' |
| *551 | *552 | S45°30'42"E | 151.33' |
| 552 | 553 | S32°23'47"E | 155.37' |
| 553 | 554 | N35°33'51"W | 90.91' |
| 554 | 555 | N47°16'51"W | 241.81' |
| 555 | 551 | S88°53'36"E | 39.33' |

*SEE CURVE TABLE

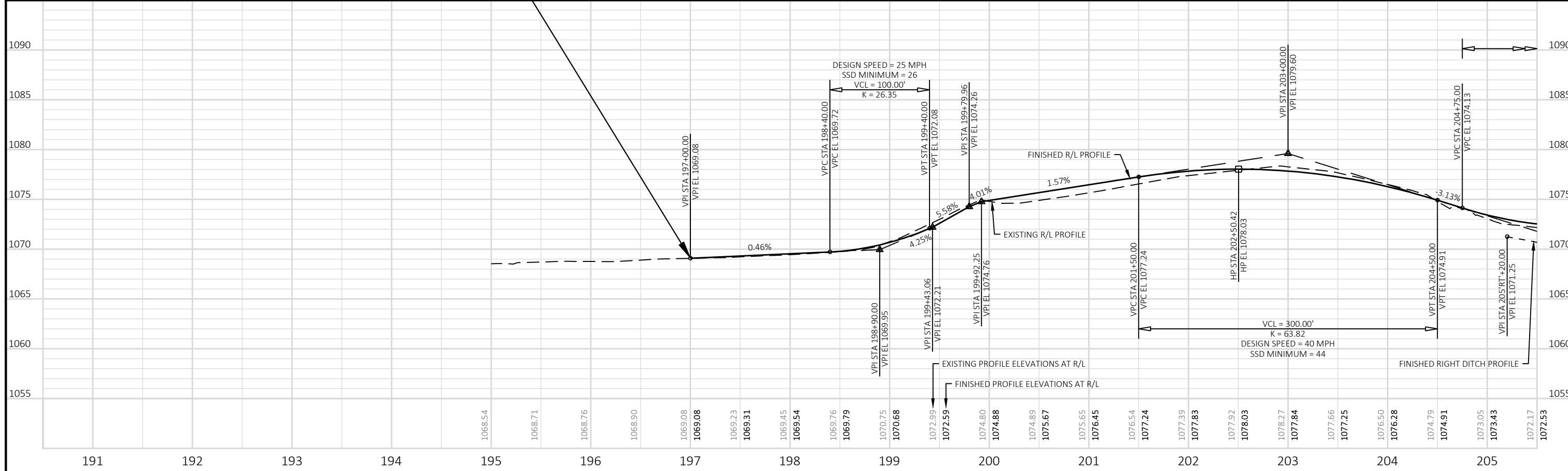
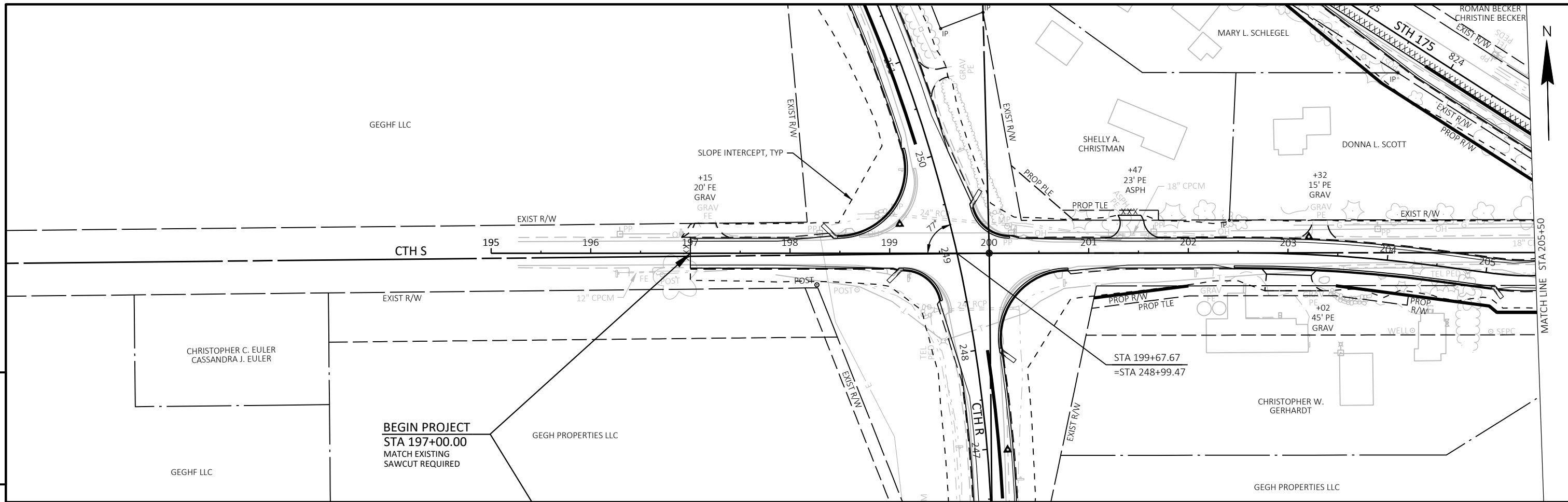
CURVE TABLE - PARCEL 11

| FROM POINT | TO POINT | RADIUS | ARC LENGTH | CHORD BEARING | CHORD LENGTH |
|------------|----------|----------|------------|---------------|--------------|
| 551 | 552 | 1482.39' | 151.40' | S45°30'42"E | 151.33' |

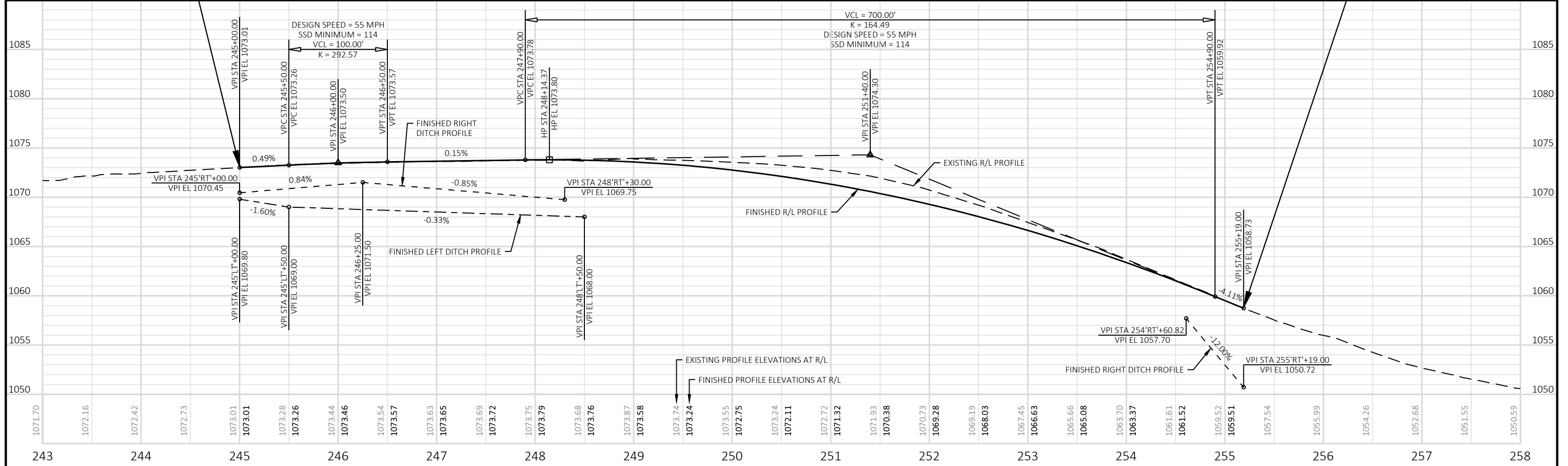
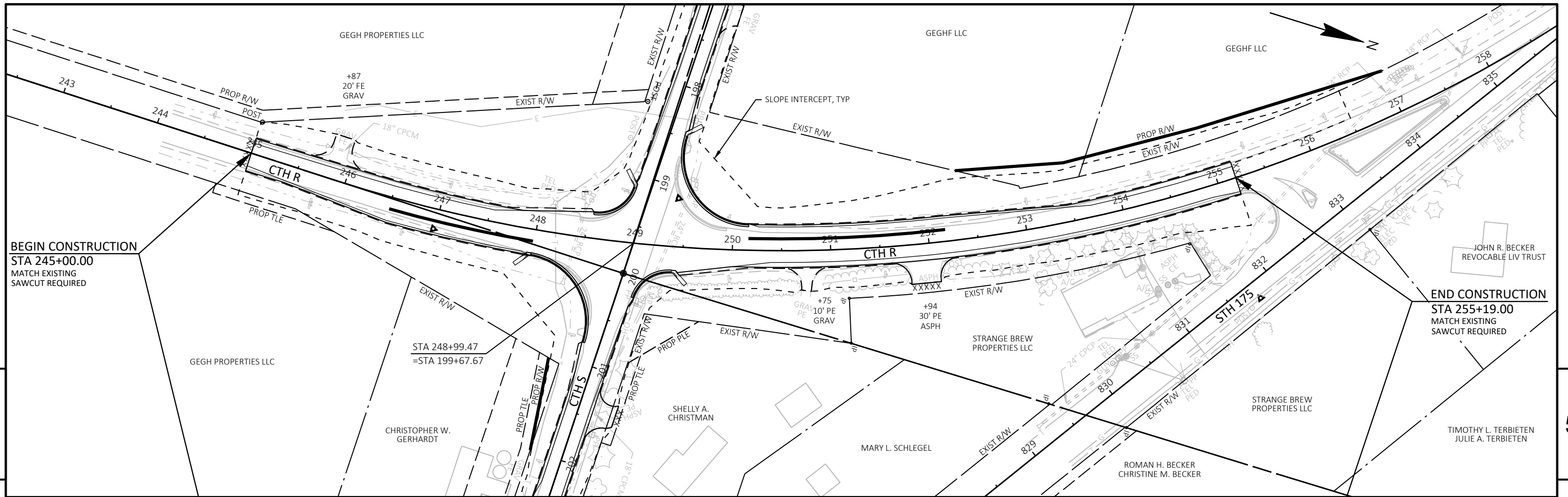
REVISED: 5/11/2023

SHEET 6 OF 6 SHEETS

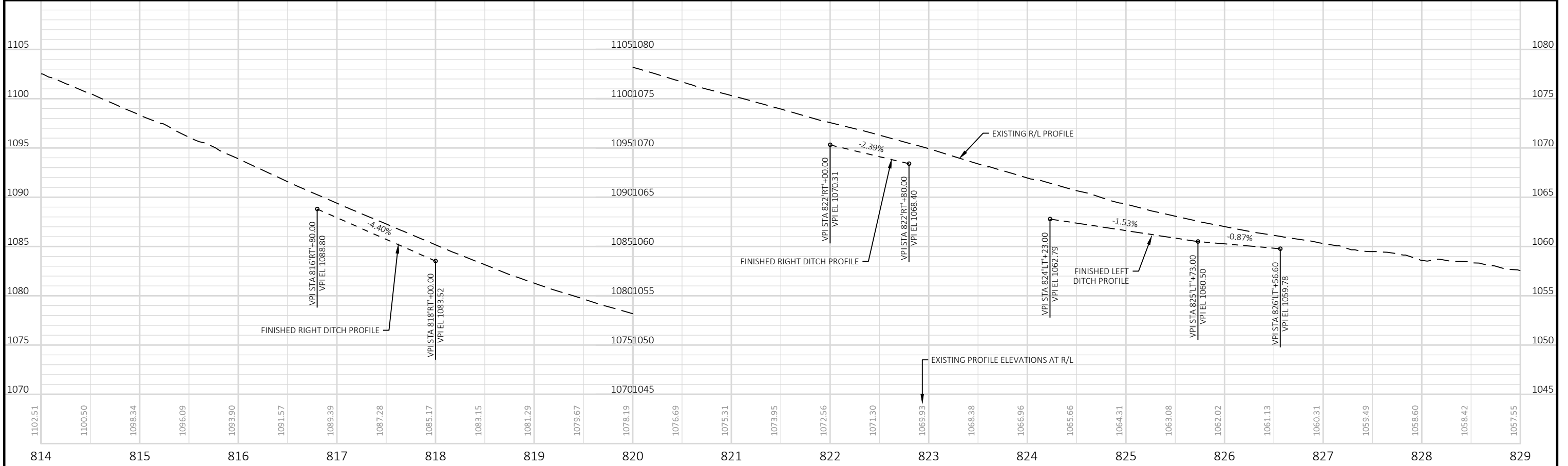
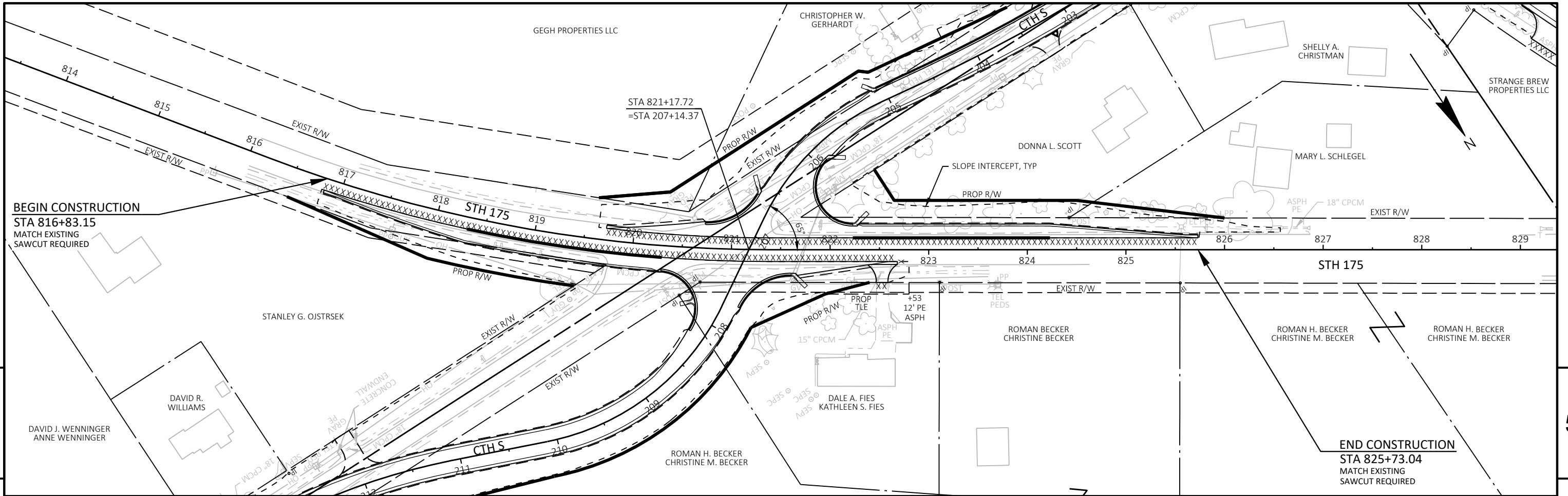
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|---------------|-----------------|-------------|----------------------------|--|------------------|
| REVISION DATE | DATE: 6/15/2023 | SCALE, FEET | HWY: STH 175, CTH S, CTH R | R/W PROJECT NUMBER: HWY 24-02 | PLAT SHEET: 4.06 |
| | GRID FACTOR N/A | | COUNTY: WASHINGTON | CONSTRUCTION PROJECT NUMBER: HWY 24-02 | PS&E SHEET: E |



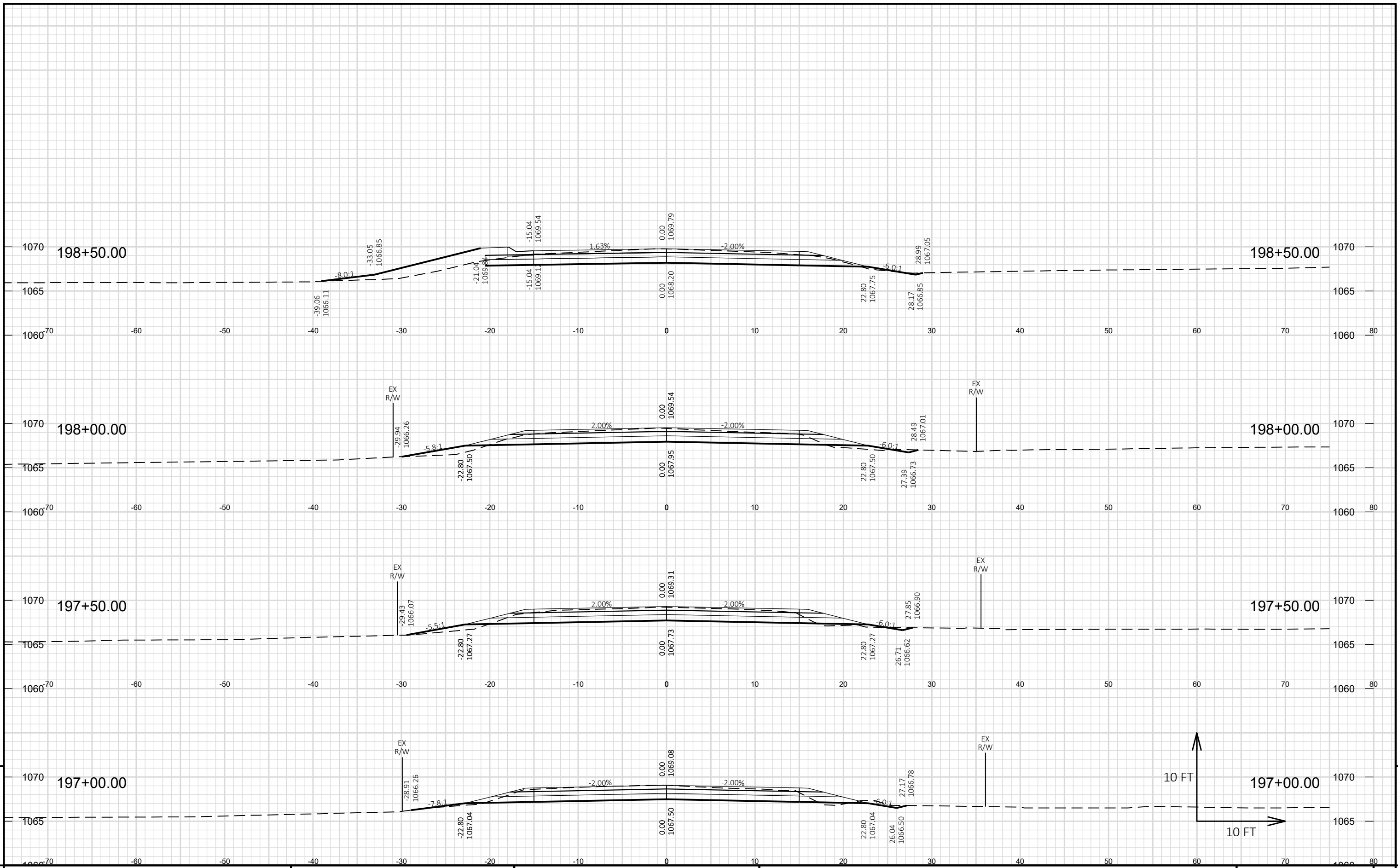
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|-------------|----------|------|-------|---------|------------|-------------------|-------|-------|---|
| PROJECT NO: | HWY24-02 | HWY: | CTH S | COUNTY: | WASHINGTON | PLAN AND PROFILE: | CTH S | SHEET | 5 |
|-------------|----------|------|-------|---------|------------|-------------------|-------|-------|---|



| | | | | | | | | | |
|-------------|----------|------|-------|---------|------------|-------------------|-------|-------|----------|
| PROJECT NO: | HWY24-02 | HWY: | CTH S | COUNTY: | WASHINGTON | PLAN AND PROFILE: | CTH R | SHEET | E |
|-------------|----------|------|-------|---------|------------|-------------------|-------|-------|----------|



| | | | | | |
|----------------------|------------|--------------------|---------------------------|-------|----------|
| PROJECT NO: HWY24-02 | HWY: CTH S | COUNTY: WASHINGTON | PLAN AND PROFILE: STH 175 | SHEET | E |
|----------------------|------------|--------------------|---------------------------|-------|----------|

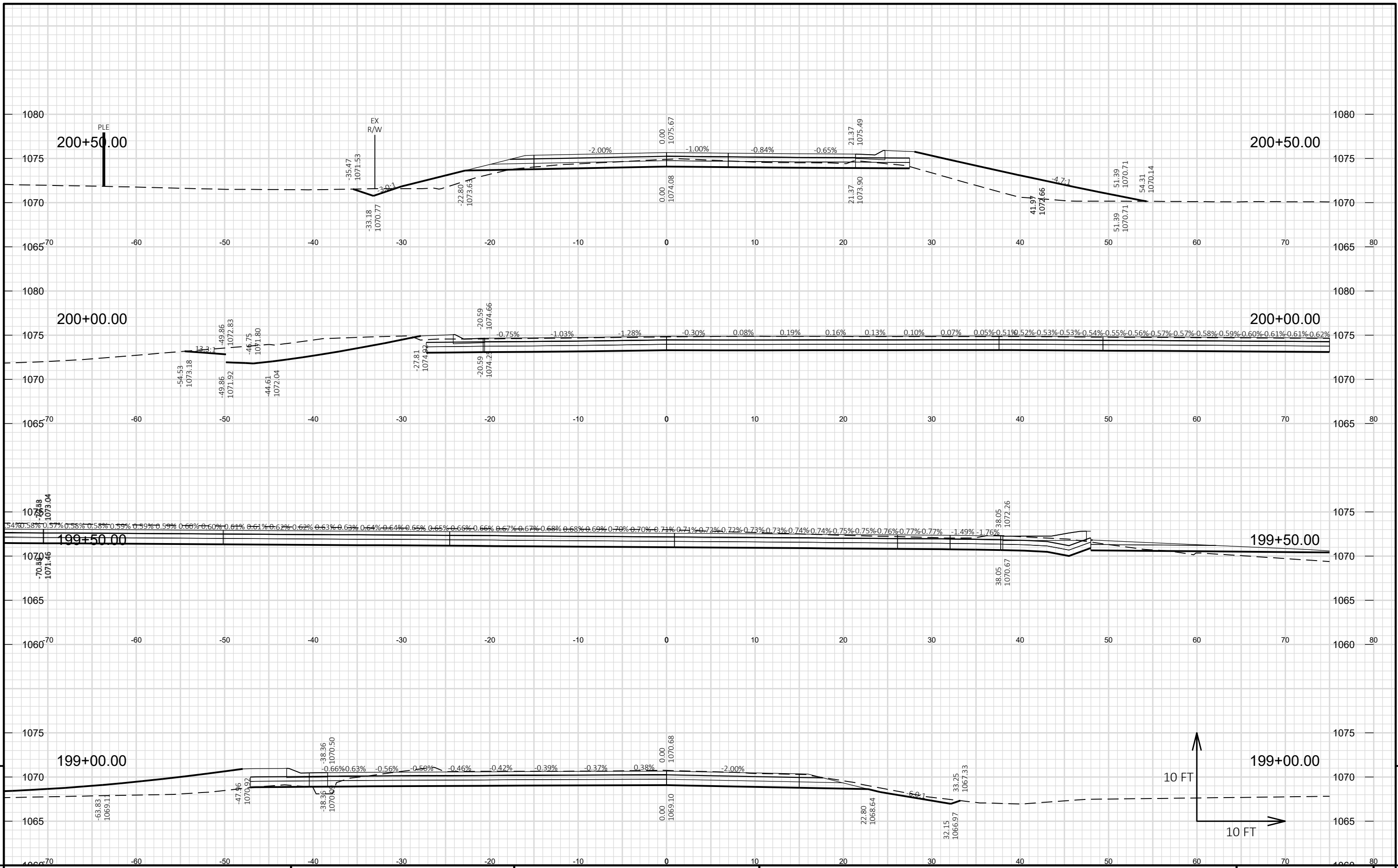


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9

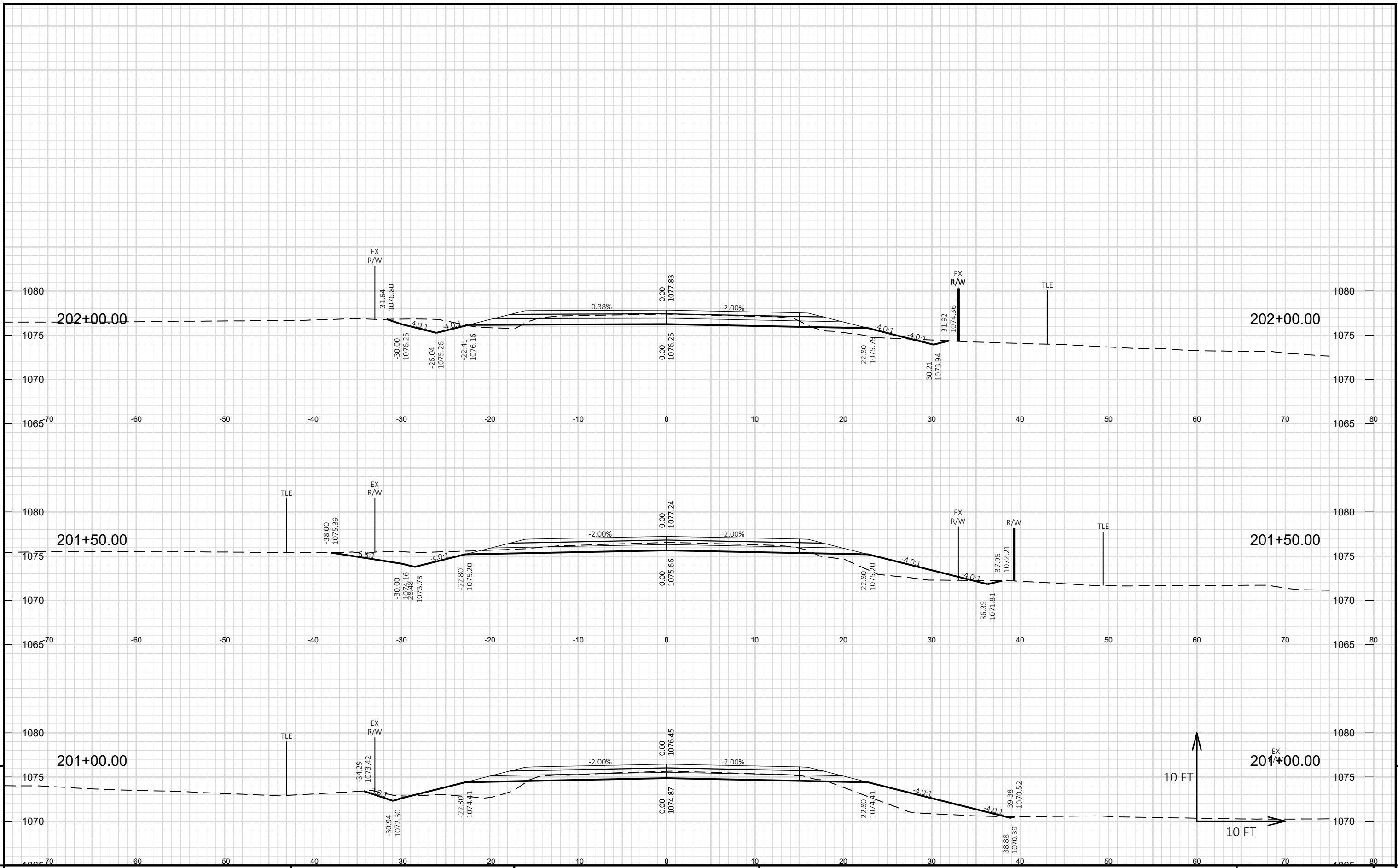
PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH S SHEET E

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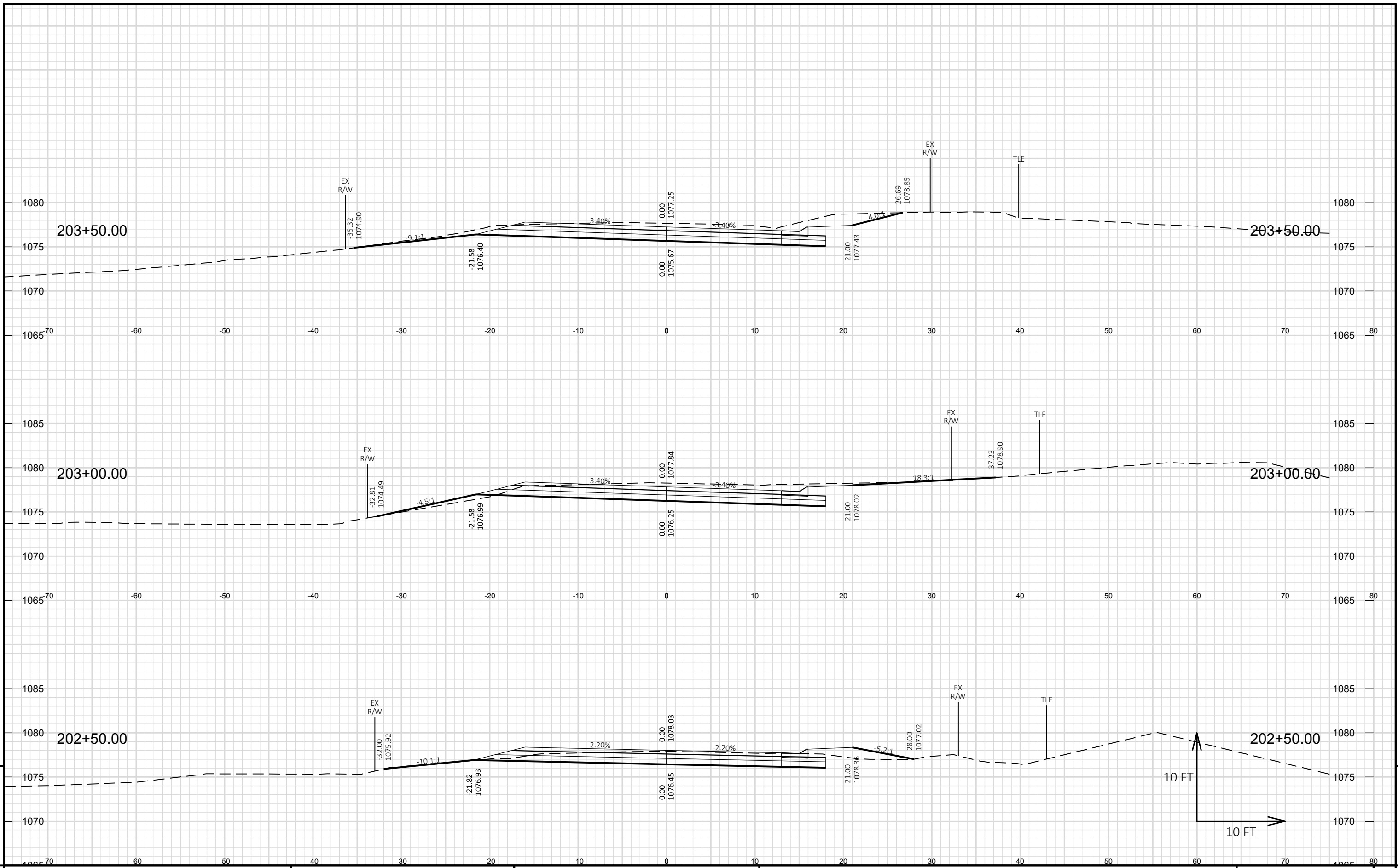


PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH S SHEET: 9

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PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH S SHEET: 9

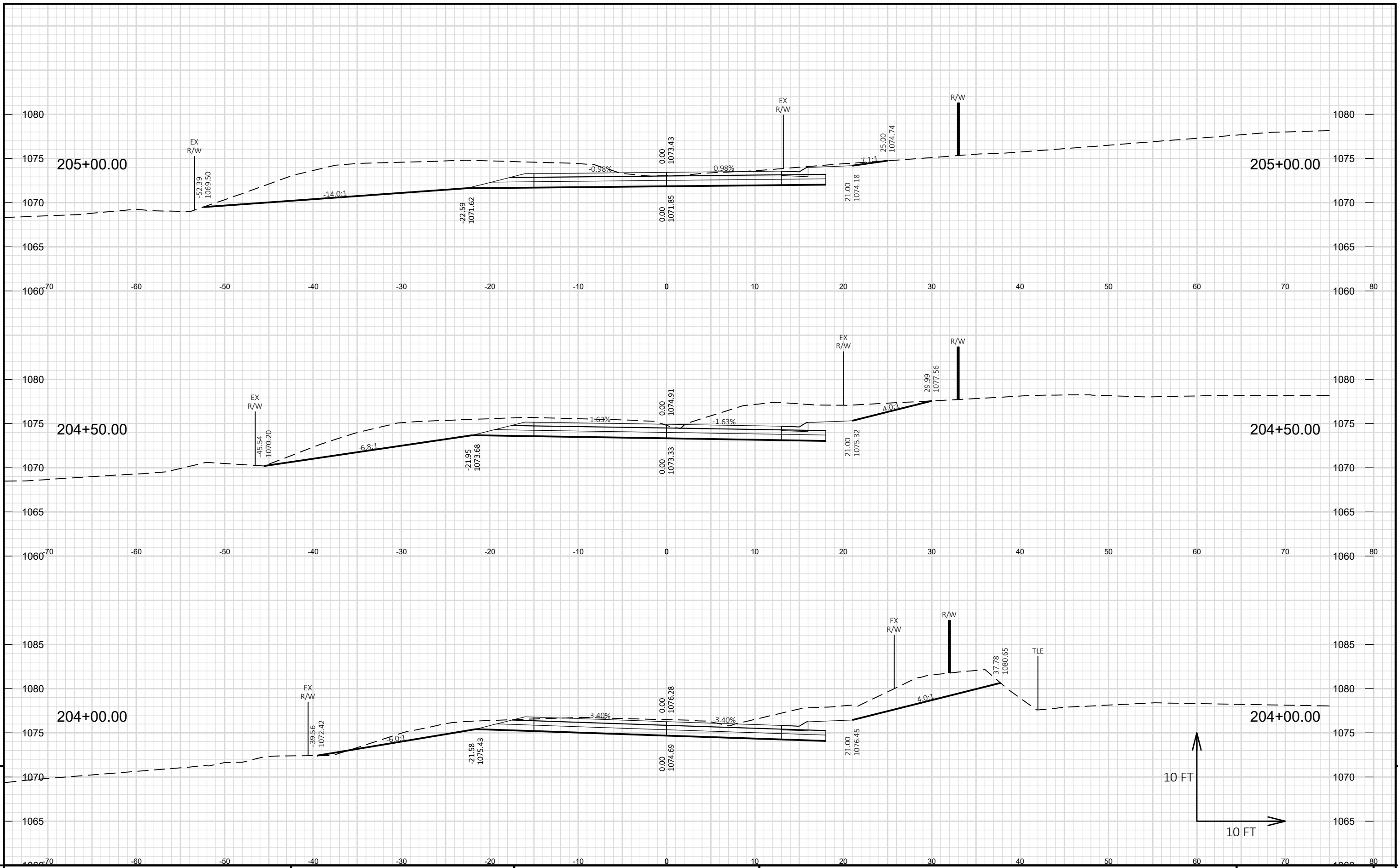


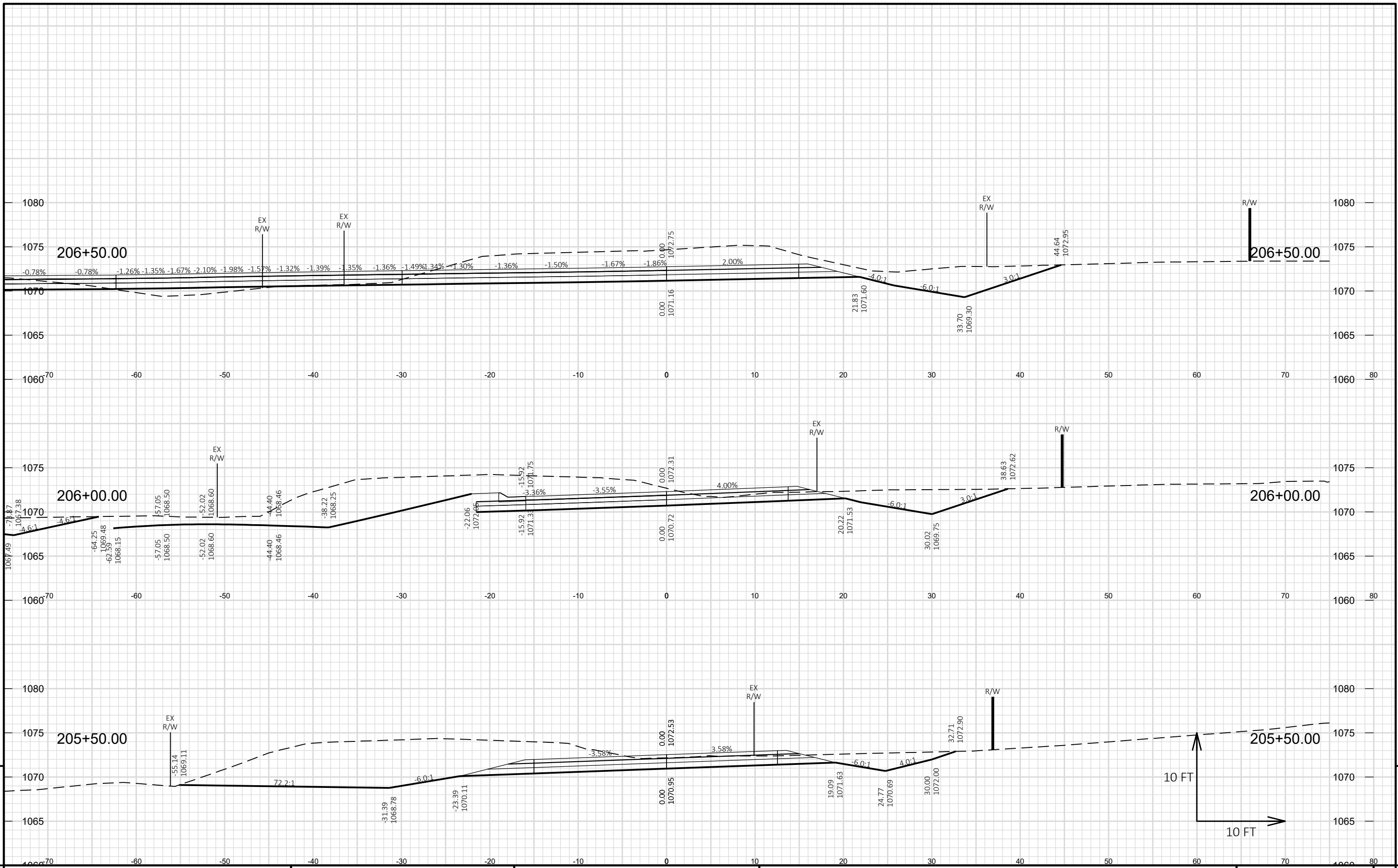
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PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH S SHEET E

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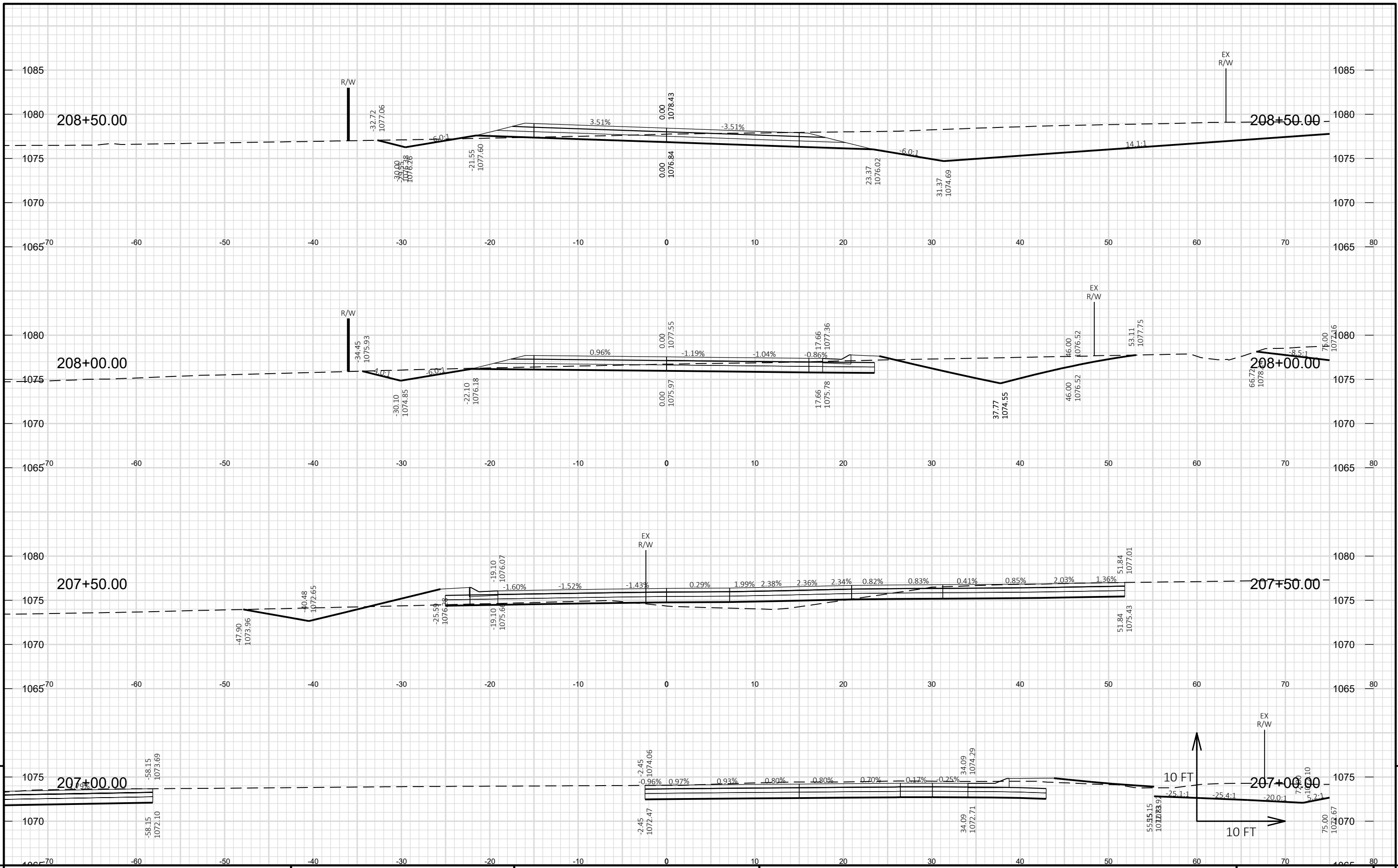




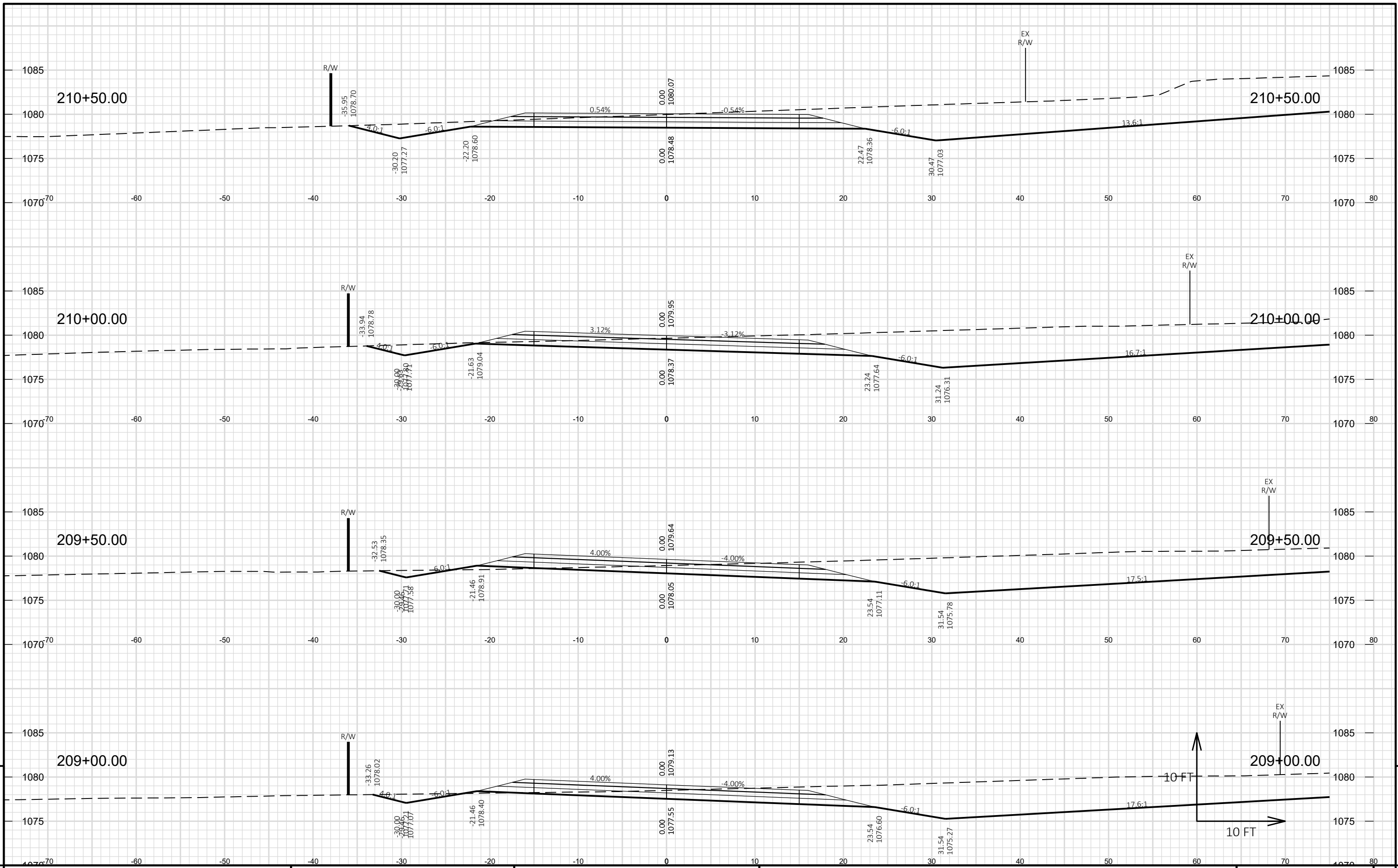
PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH S SHEET E

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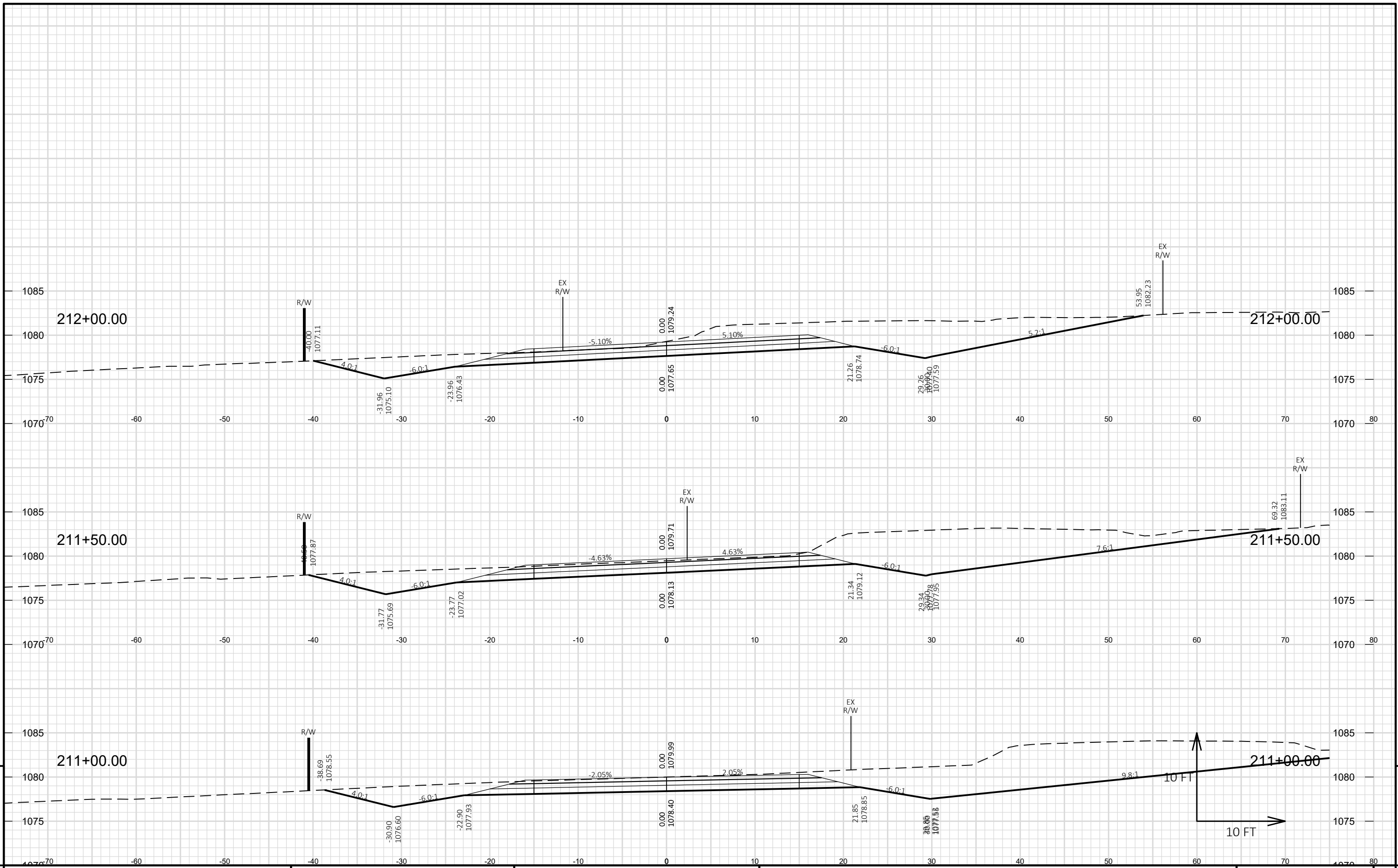
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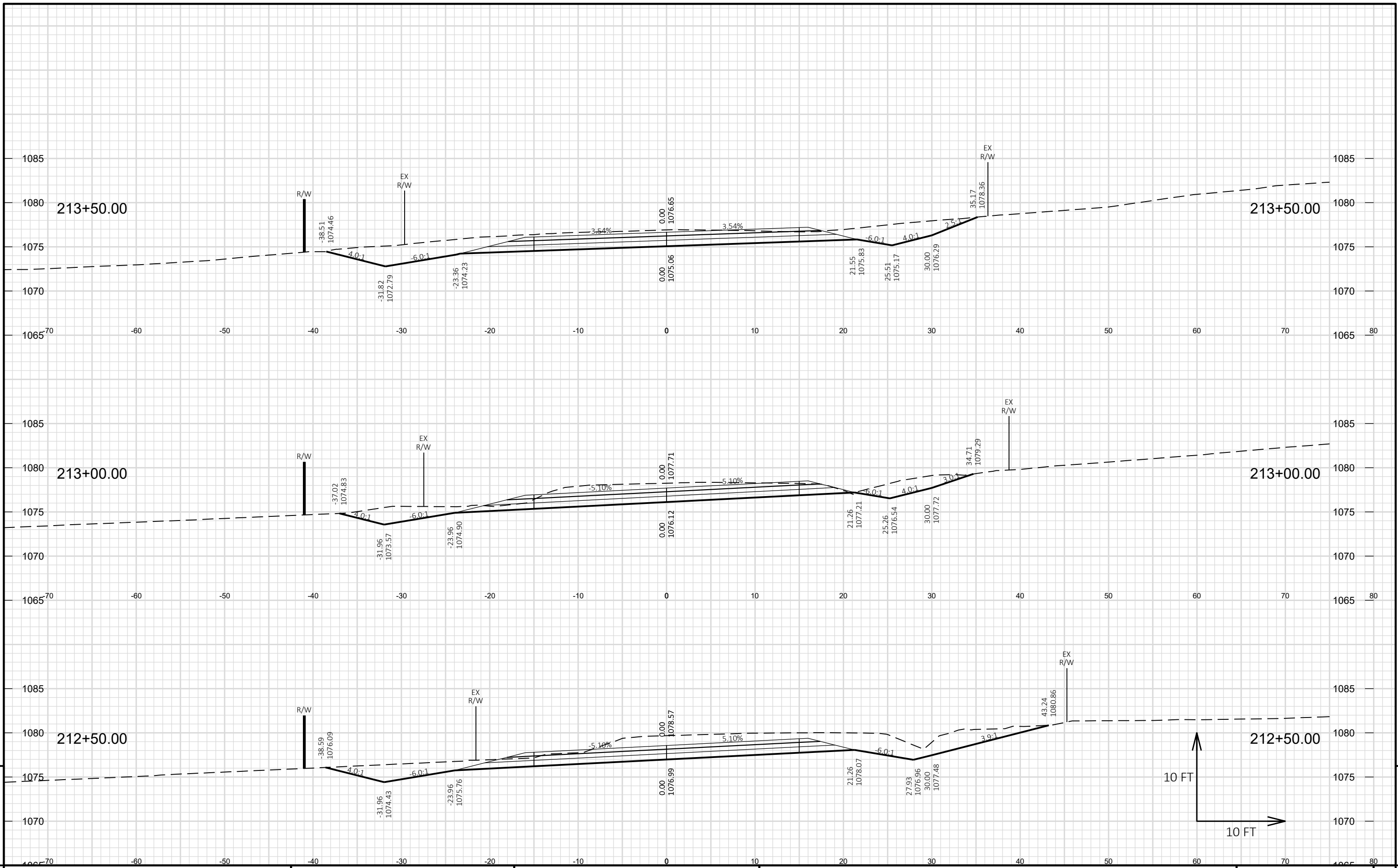
PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH S SHEET: 9



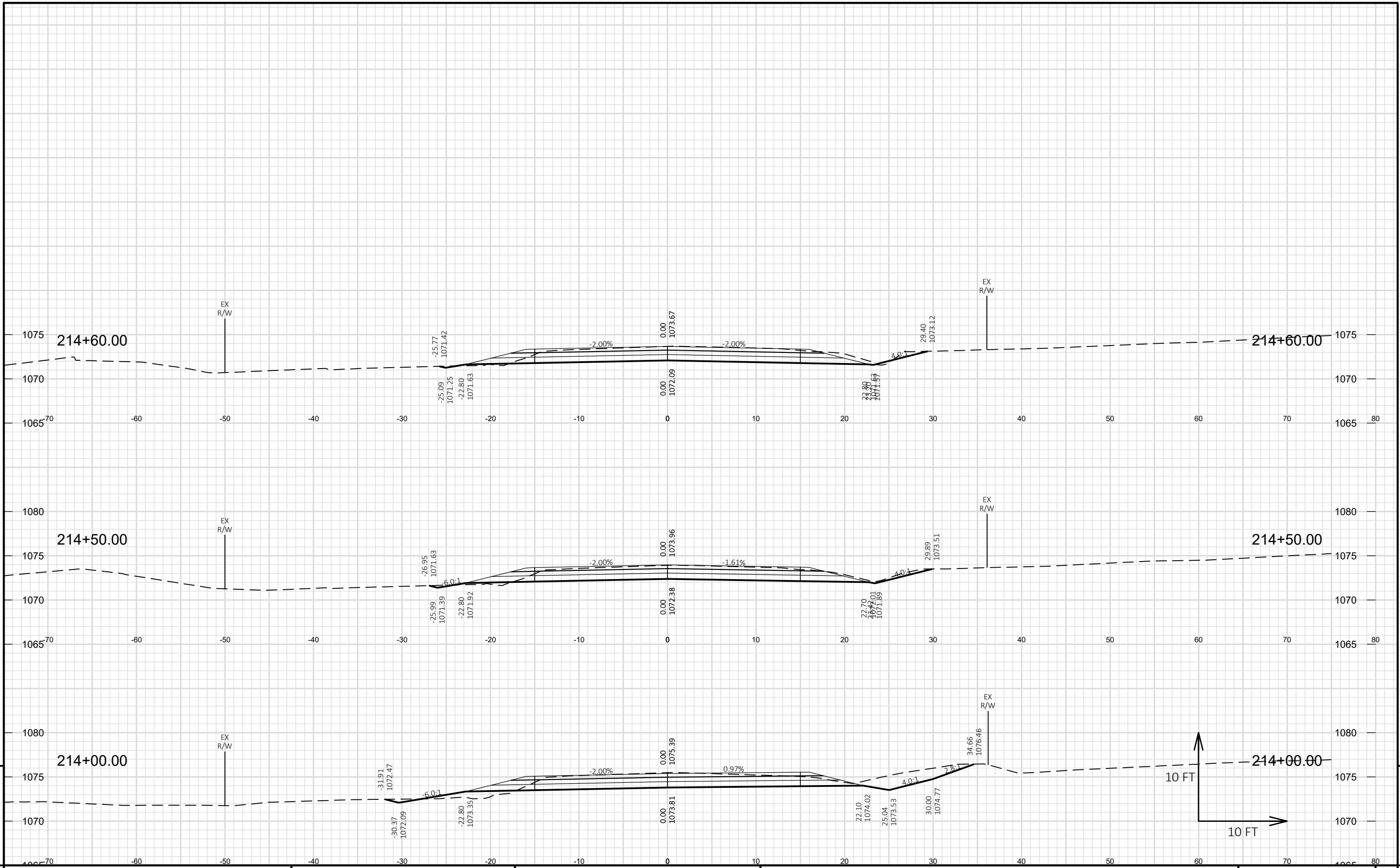
PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH S SHEET E



PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH S SHEET E



PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH S SHEET E



PROJECT NO: HWY24-02

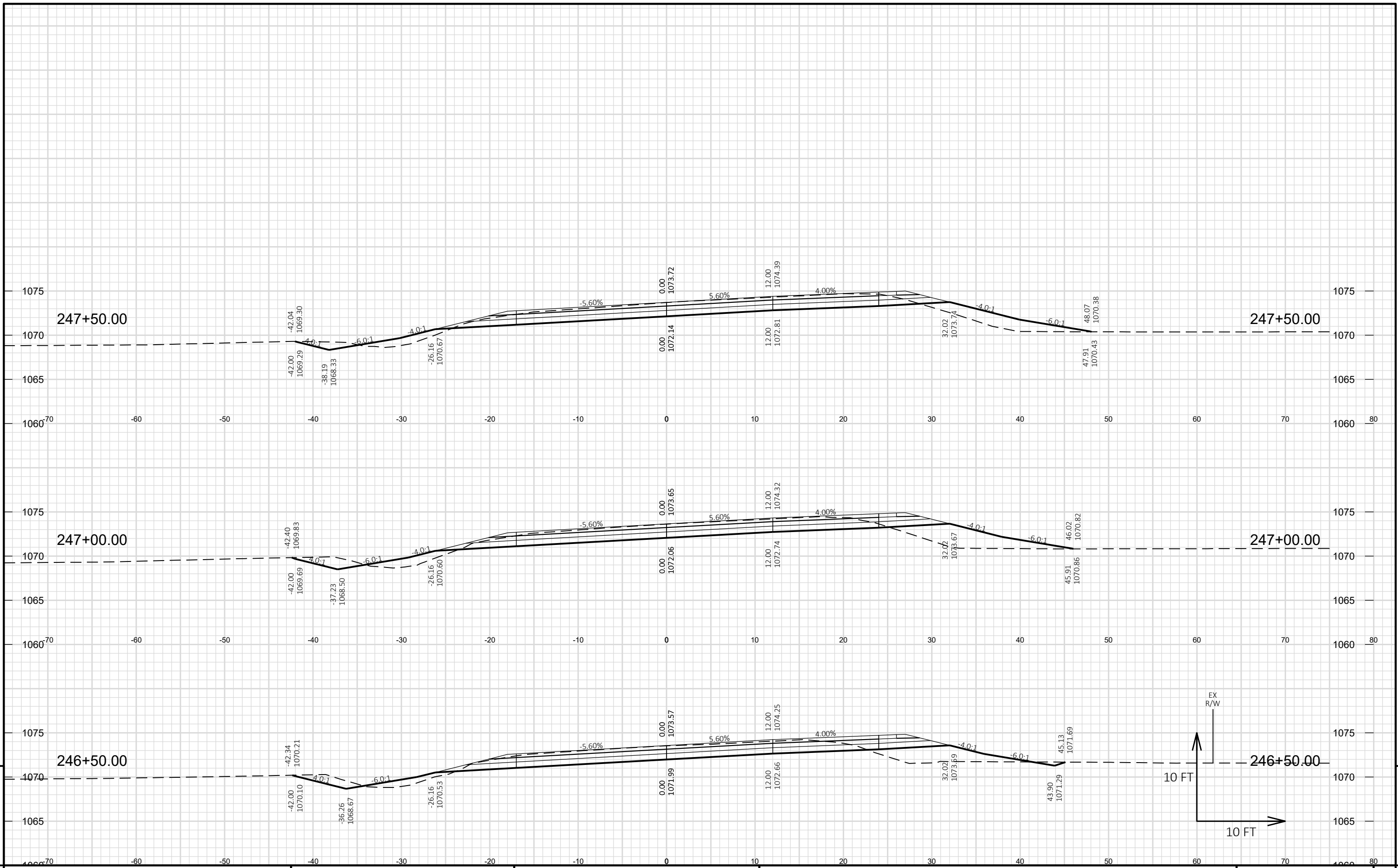
HWY: CTH S

COUNTY: WASHINGTON

CROSS SECTIONS: CTH S

SHEET

E



PROJECT NO: HWY24-02

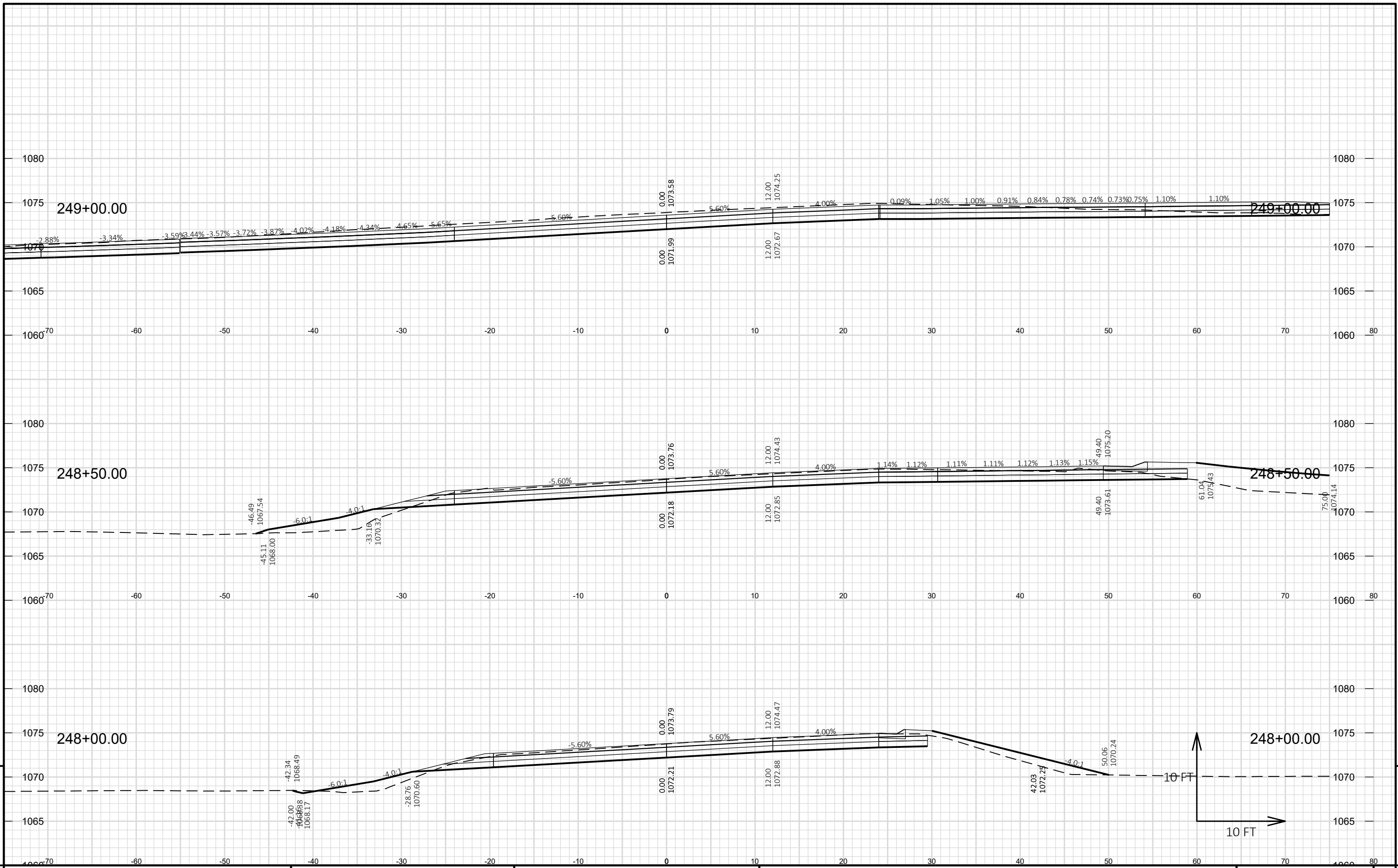
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COUNTY: WASHINGTON

CROSS SECTIONS: CTH R

SHEET

E



PROJECT NO: HWY24-02

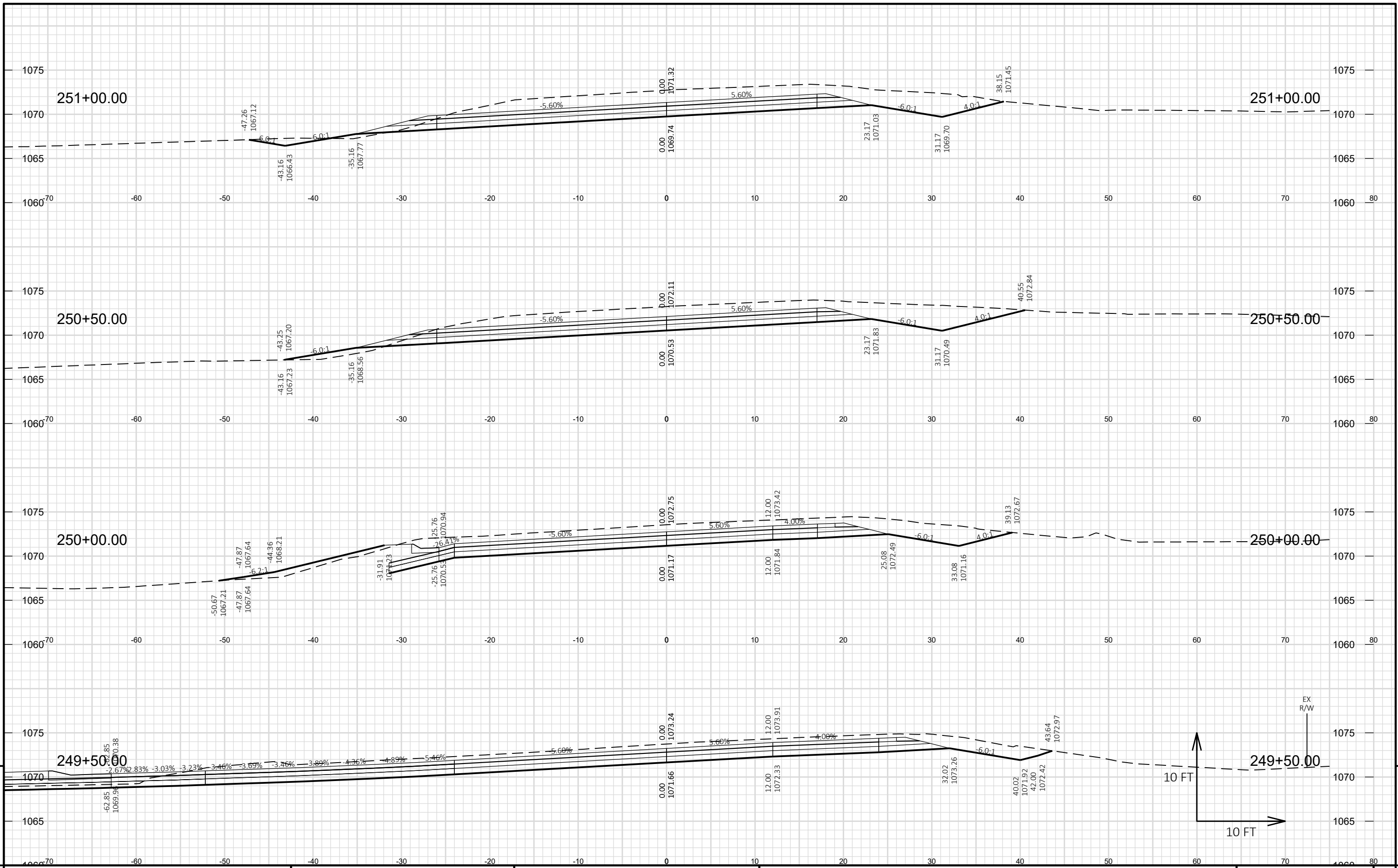
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COUNTY: WASHINGTON

CROSS SECTIONS: CTH R

SHEET

E



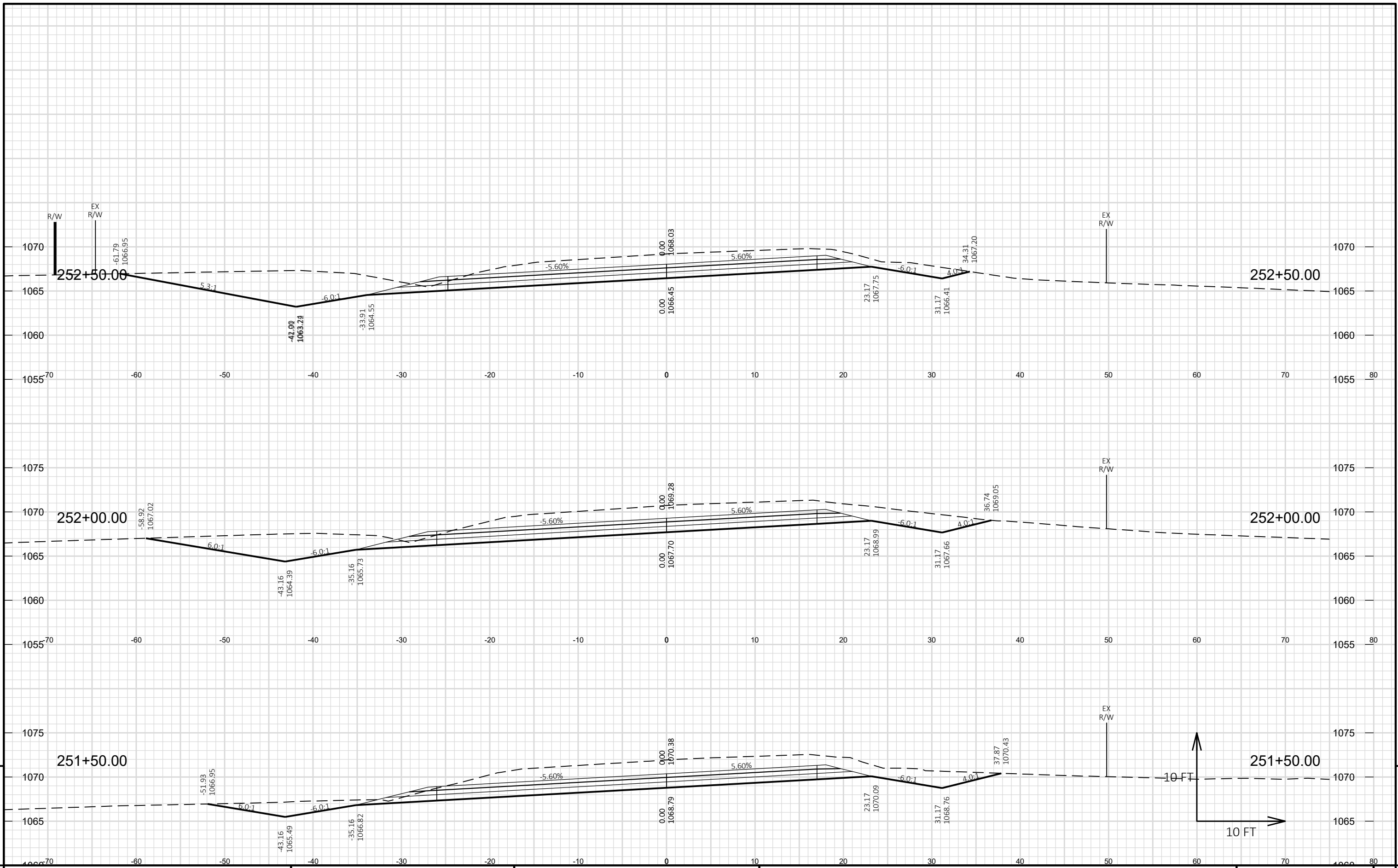
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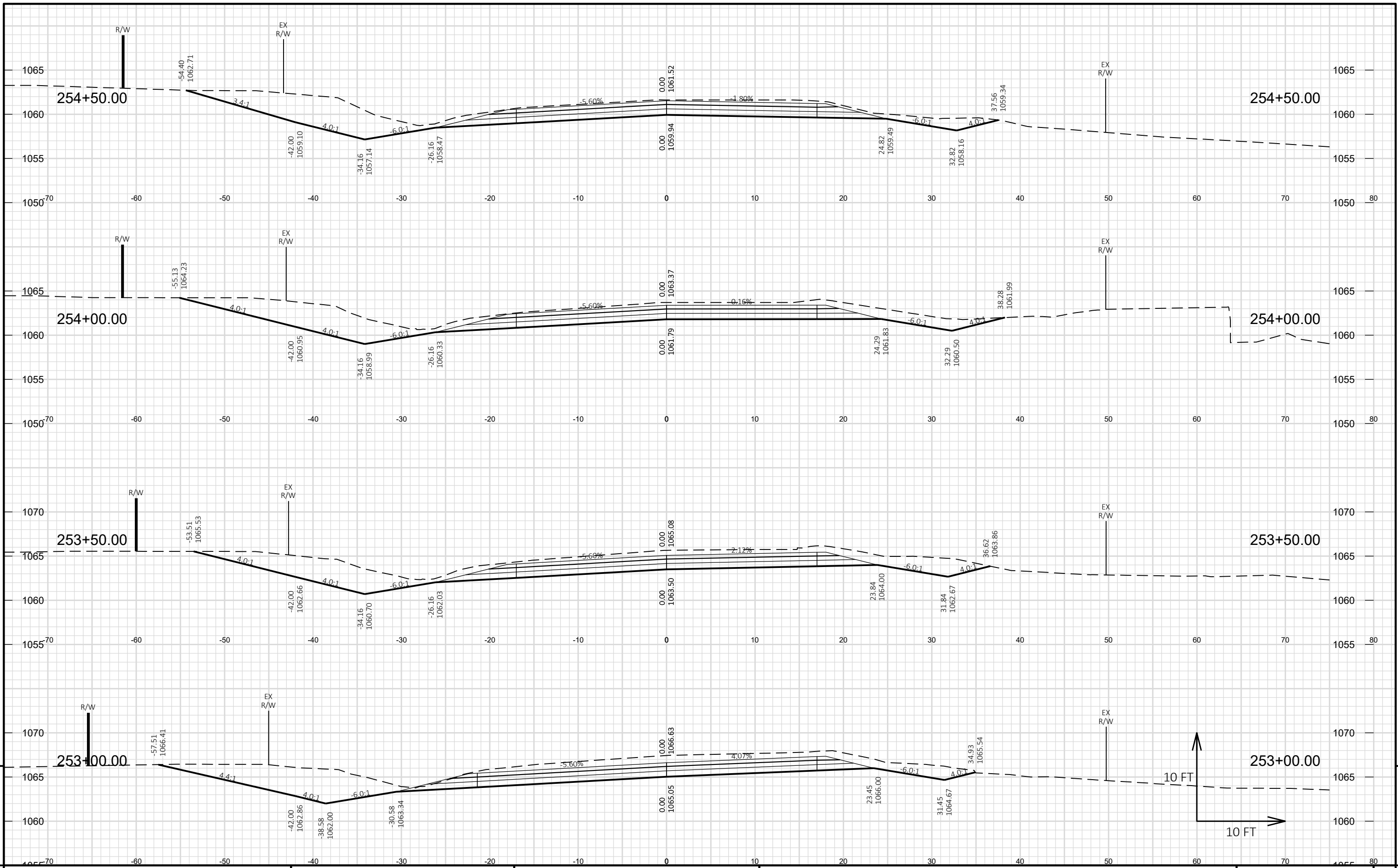
PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH R SHEET E

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LAYOUT NAME - 203-CTHR

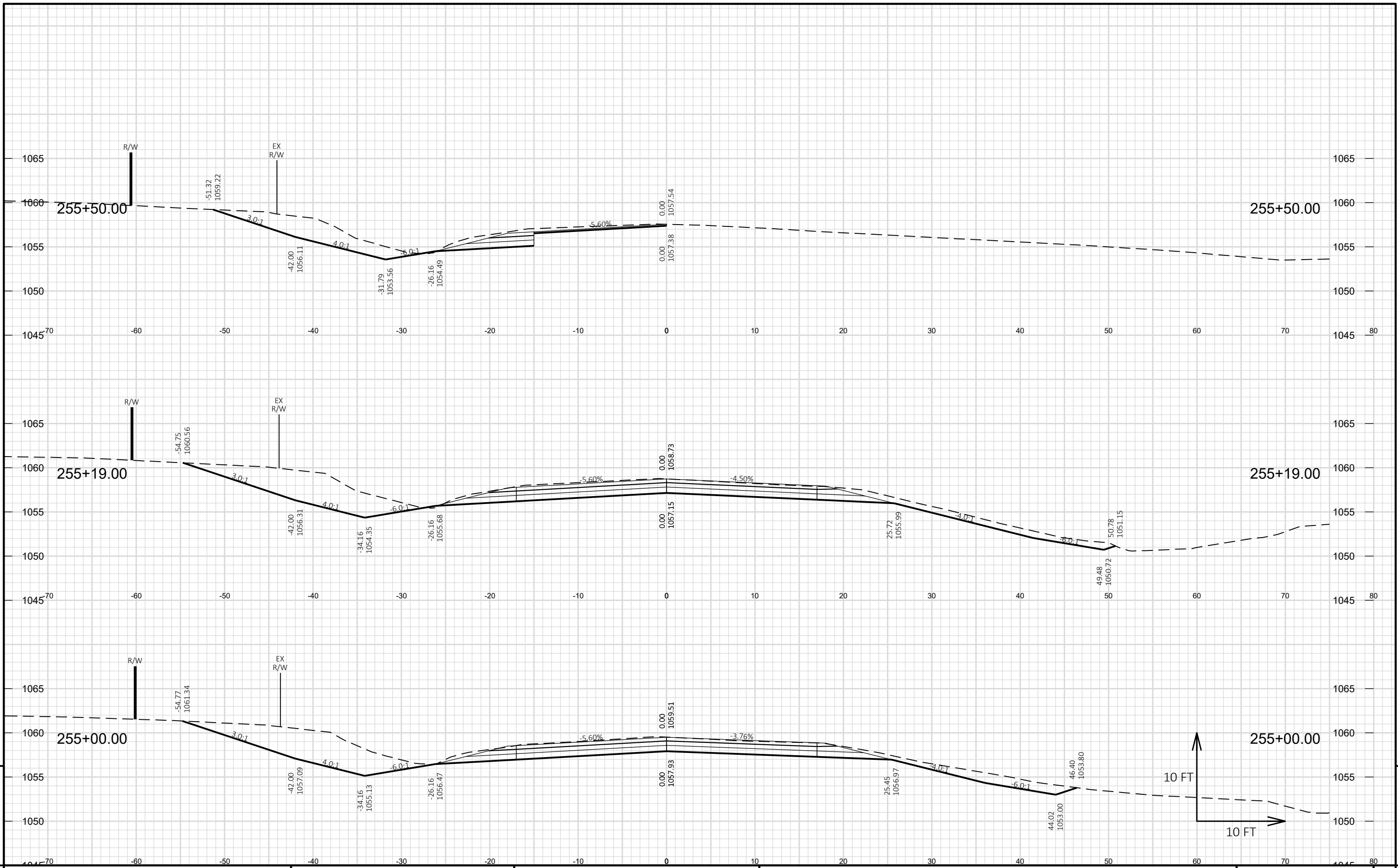


PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH R SHEET: 9



PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH R SHEET E

FILE NAME: S:\CURRPRO\WASHINCO\CTH S-STH 175\CIVIL3D\CTH S-STH 175\SHEETS\S-175-090201-XS.DWG PLOT DATE: 5/10/2023 2:54 PM PLOT BY: BENJAMIN OITZINGER PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49



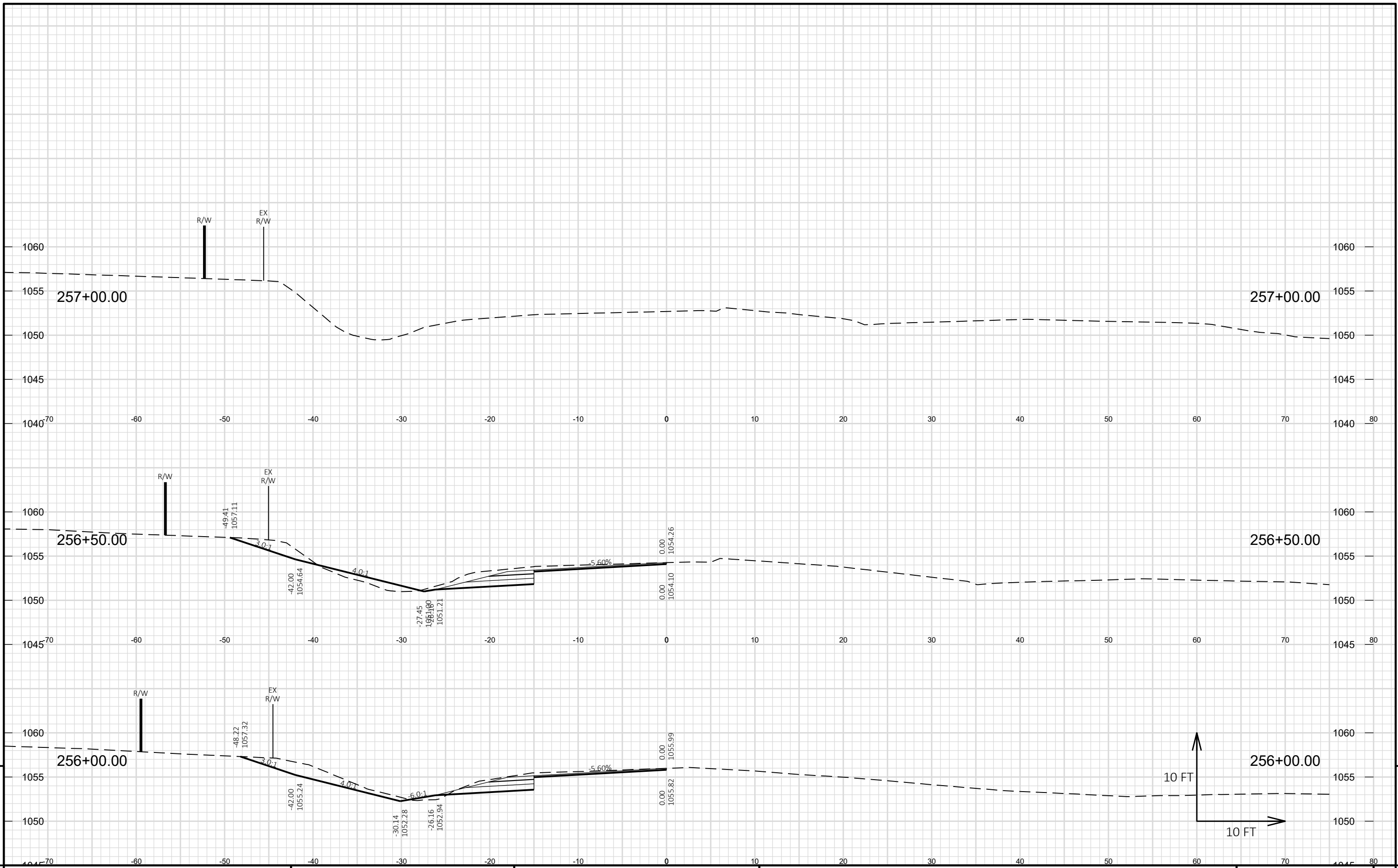
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PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH R SHEET E

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LAYOUT NAME - 206-CTHR



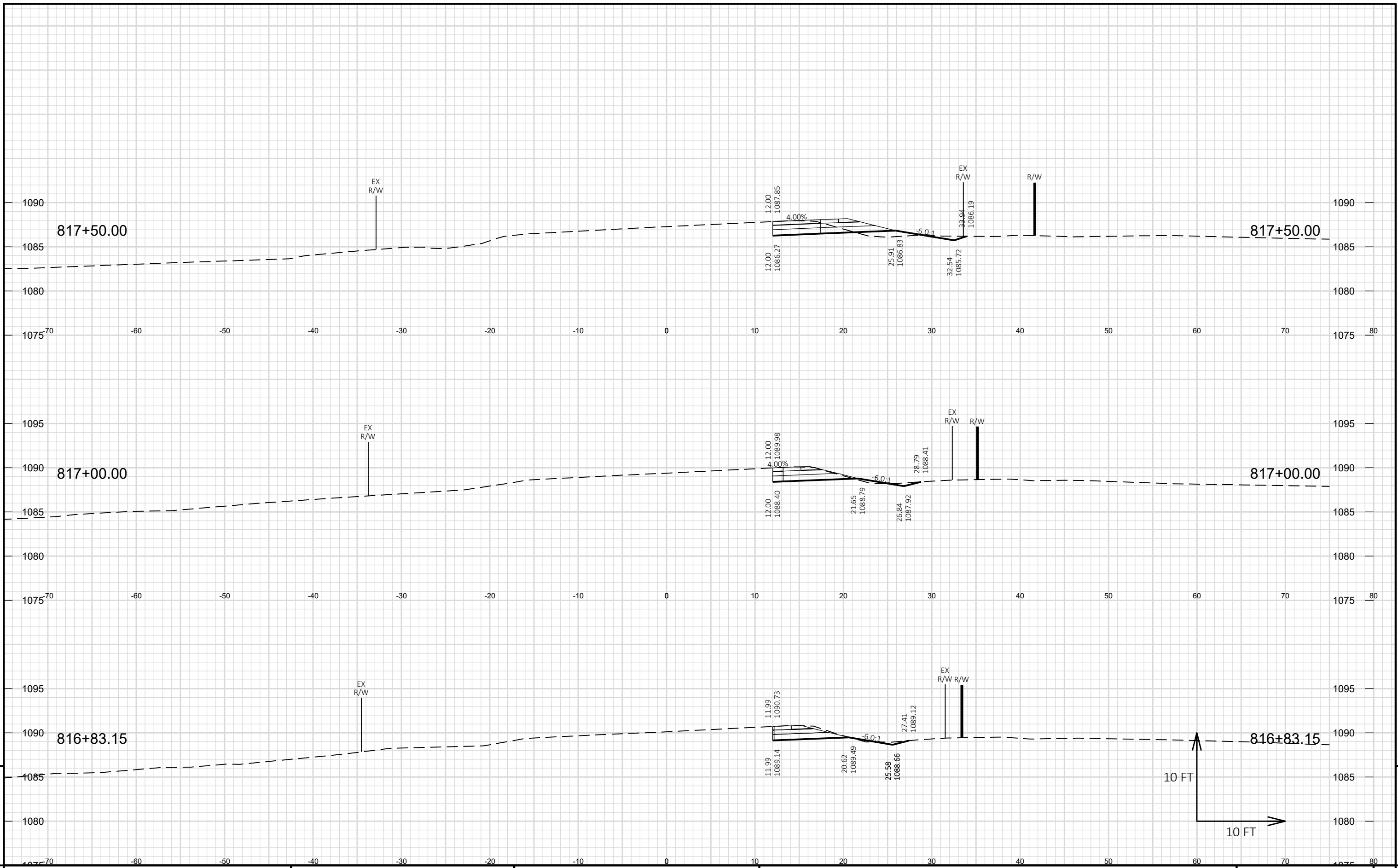
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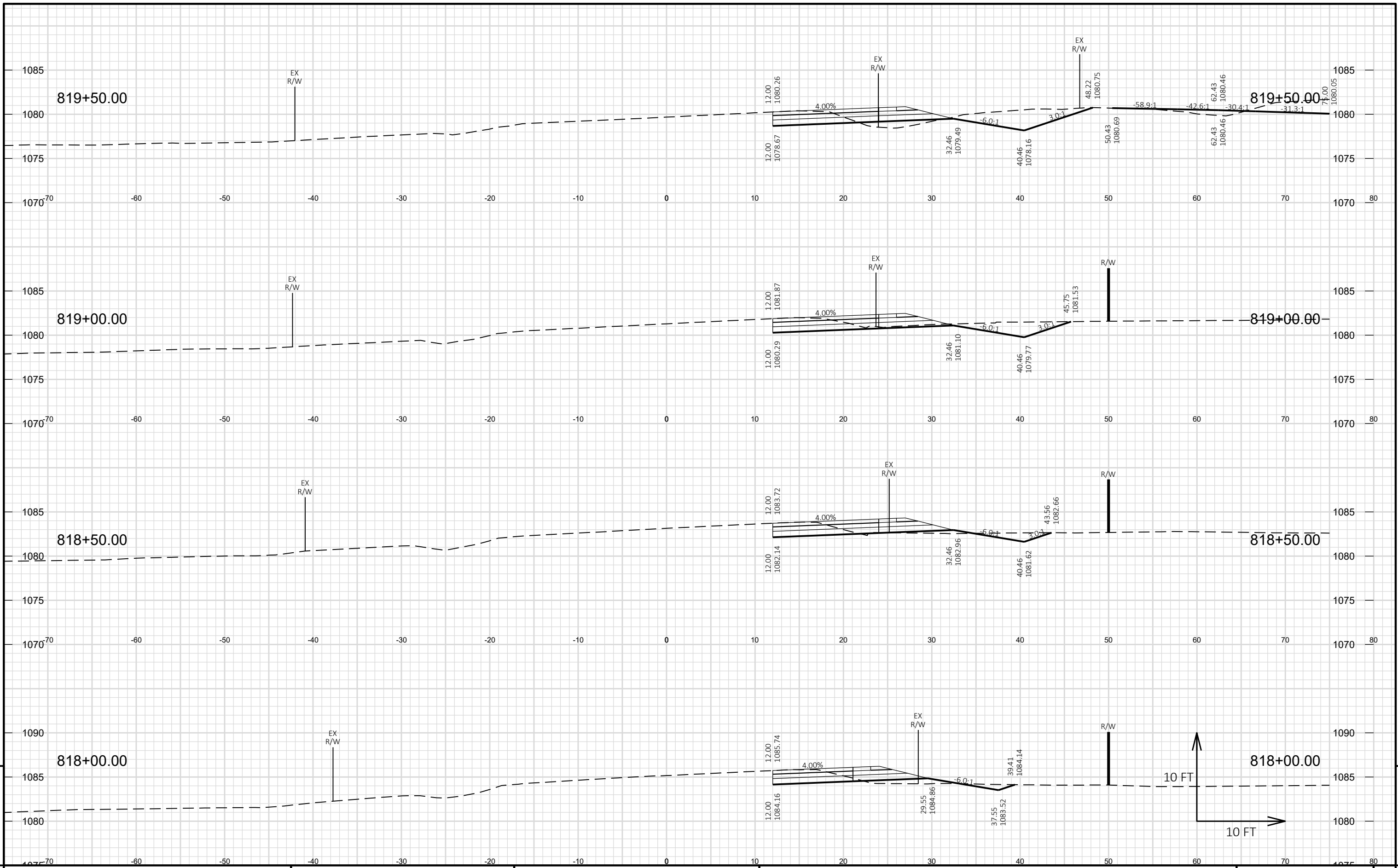
PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: CTH R SHEET E

FILE NAME : S:\CURRPROJ\WASHINCO\CTH S-STH 175\CIVIL3D\CTH S-STH 175\SHEETS\S-175-090201-XS.DWG PLOT DATE : 5/10/2023 2:54 PM PLOT BY : BENJAMIN OITZINGER PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

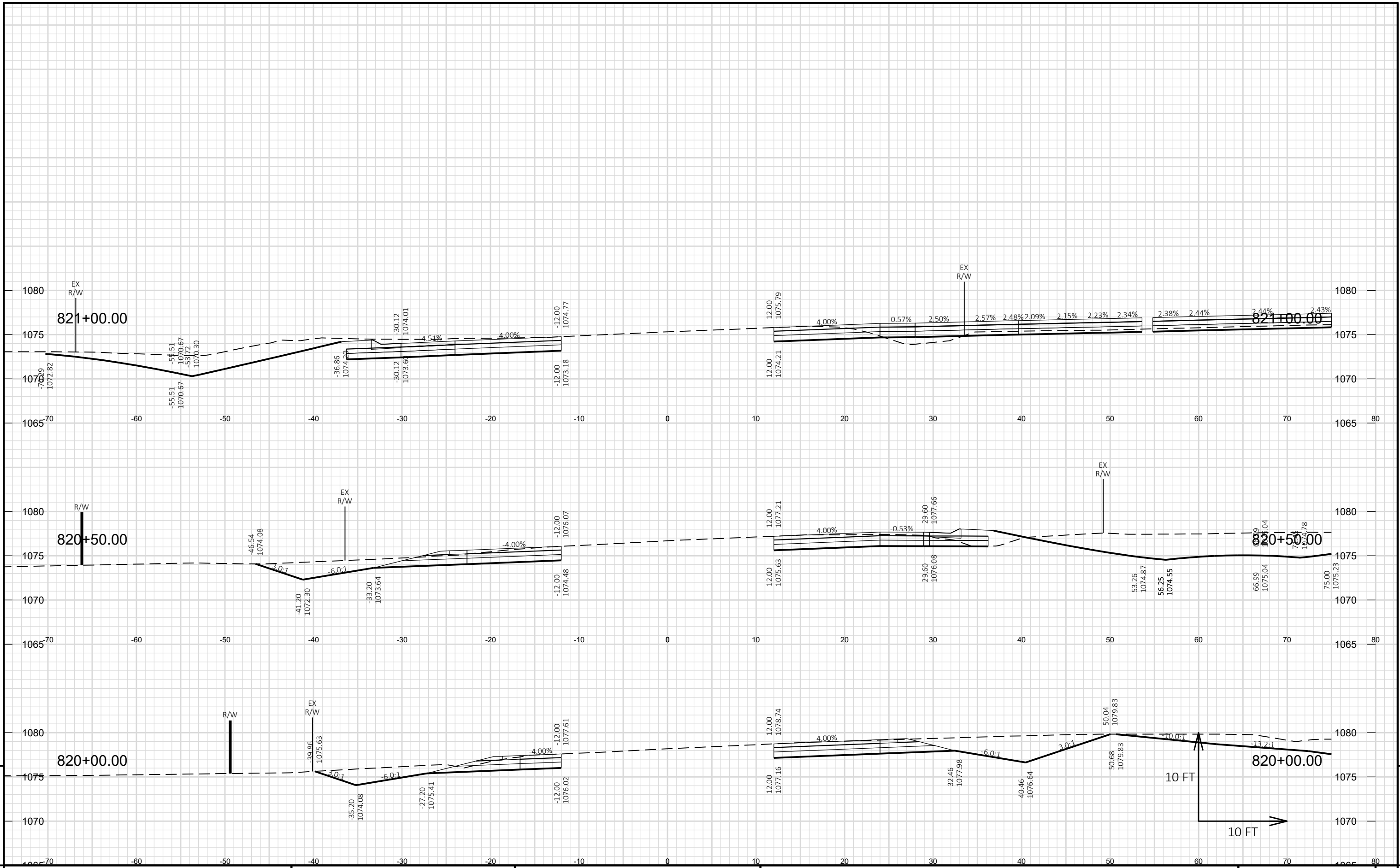
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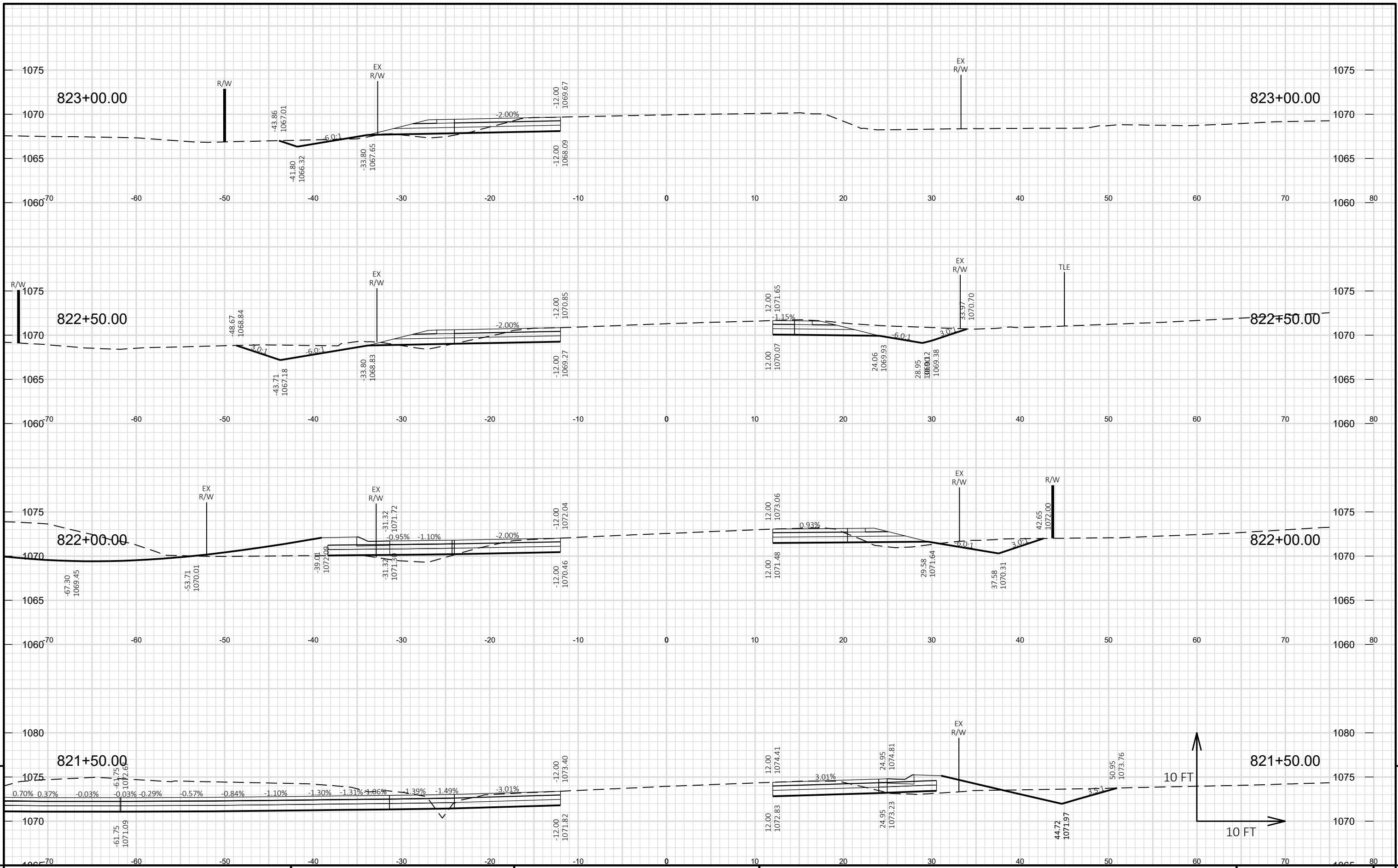
PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: STH 175 SHEET E



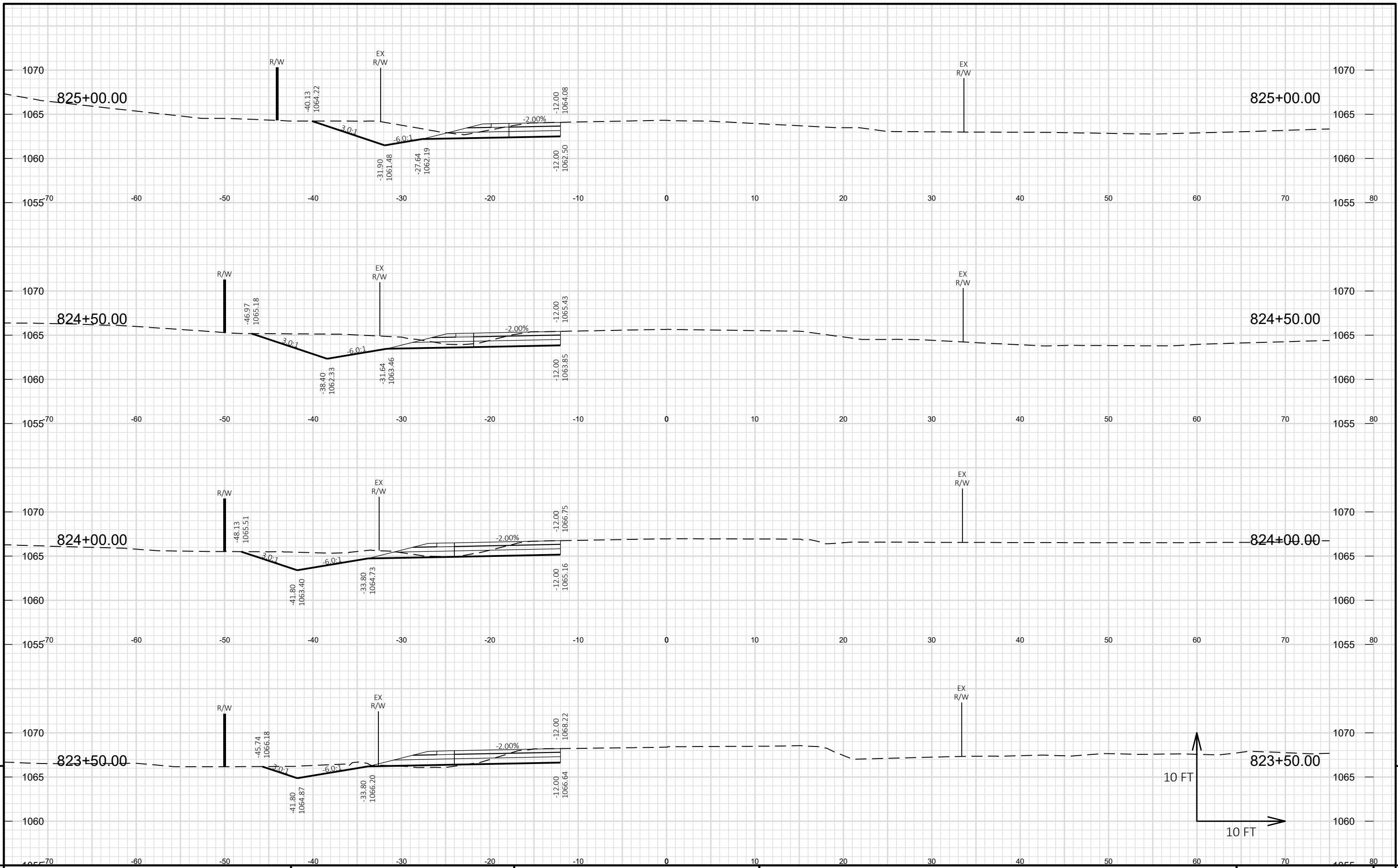
PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: STH 175 SHEET E



PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: STH 175 SHEET E



PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: STH 175 SHEET: 9



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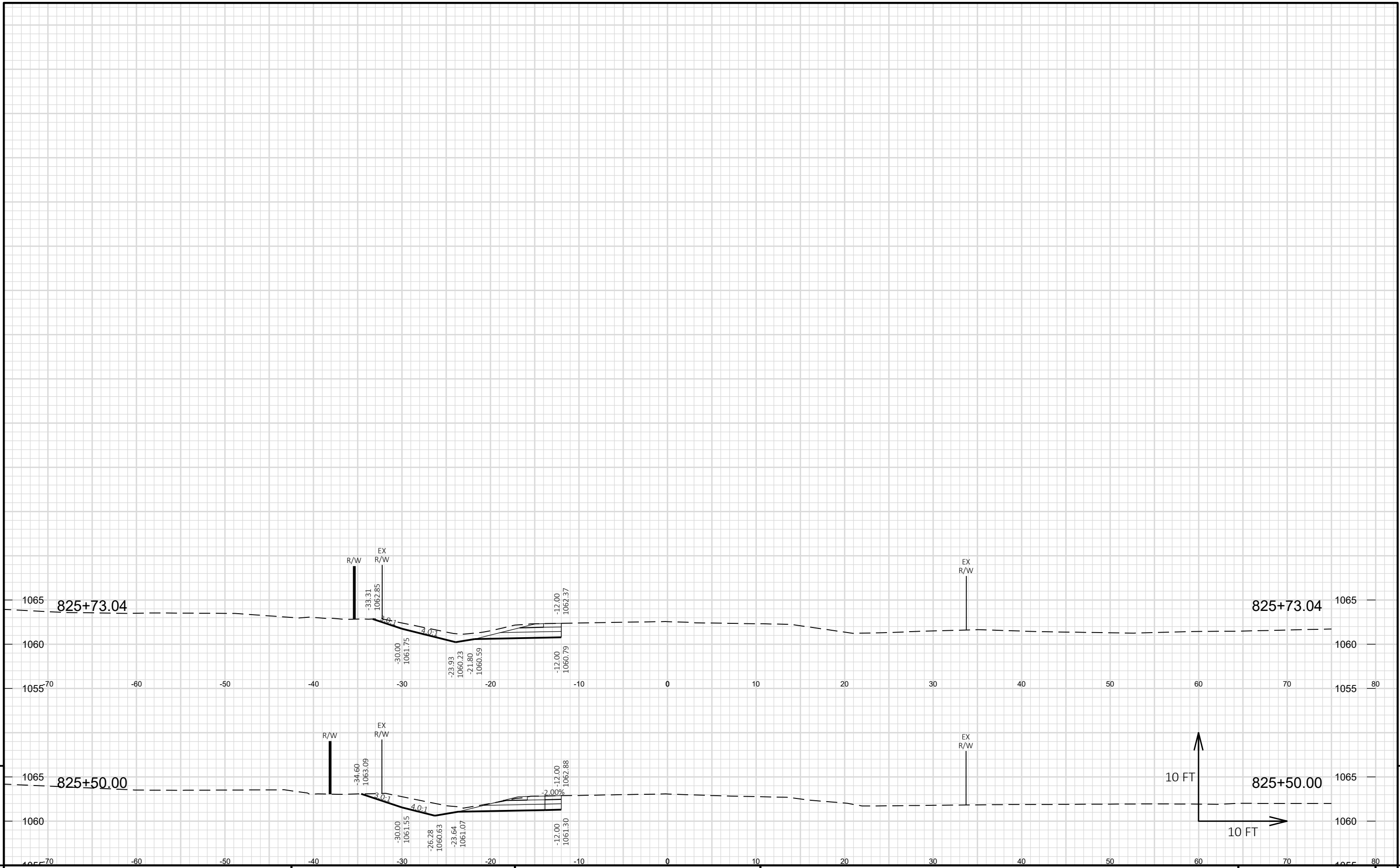
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10 FT

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PROJECT NO: HWY24-02 HWY: CTH S COUNTY: WASHINGTON CROSS SECTIONS: STH 175 SHEET E

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PROJECT NO: HWY24-02

HWY: CTH S

COUNTY: WASHINGTON

CROSS SECTIONS: STH 175

SHEET

E